



City of Tampa

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ADDENDUM 1

Via E-Mail

DATE: December 6, 2019

Contract 20-C-00002; Comprehensive Infrastructure for Tampa's Neighborhoods - Design-Build

Submitters on the above referenced project are hereby notified that the following addendum is made to the RFQ. Responses submitted shall conform to this notice.

Item 1: Attached are additional reference documents.

All other provisions of the RFQ not in conflict with this Addendum shall remain in full force and effect. Questions are to be e-mailed to ContractAdministration@tampagov.net.

Jim Greiner

Jim Greiner, P.E., Contract Management Supervisor

Project: North Boulevard Concept Plans

From Busch Boulevard to Country Club Drive

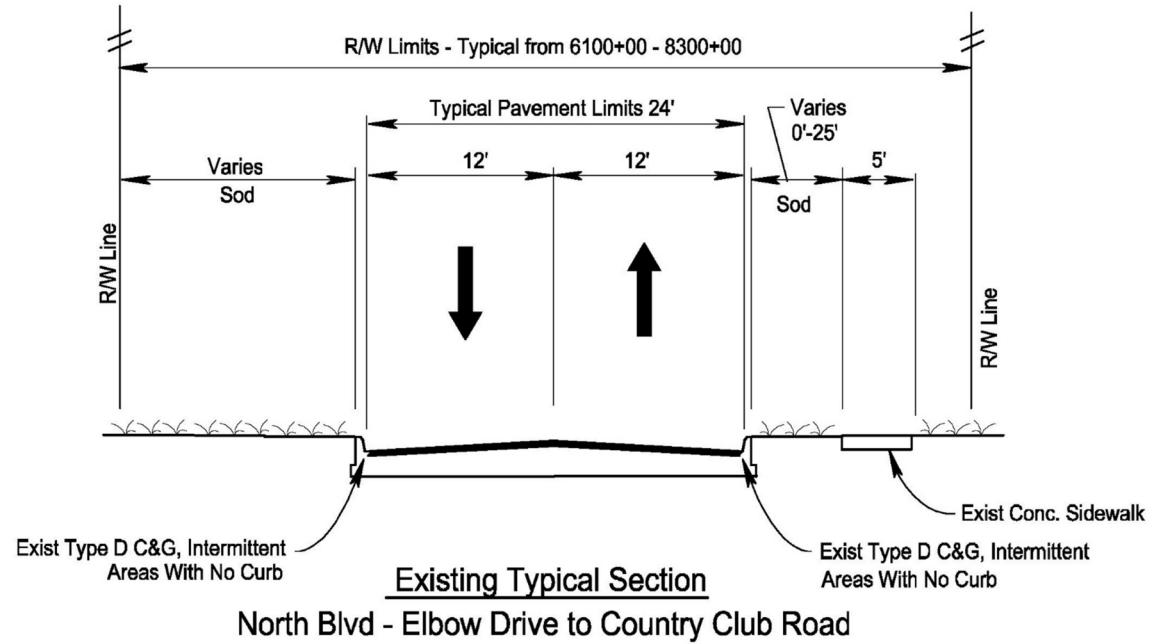
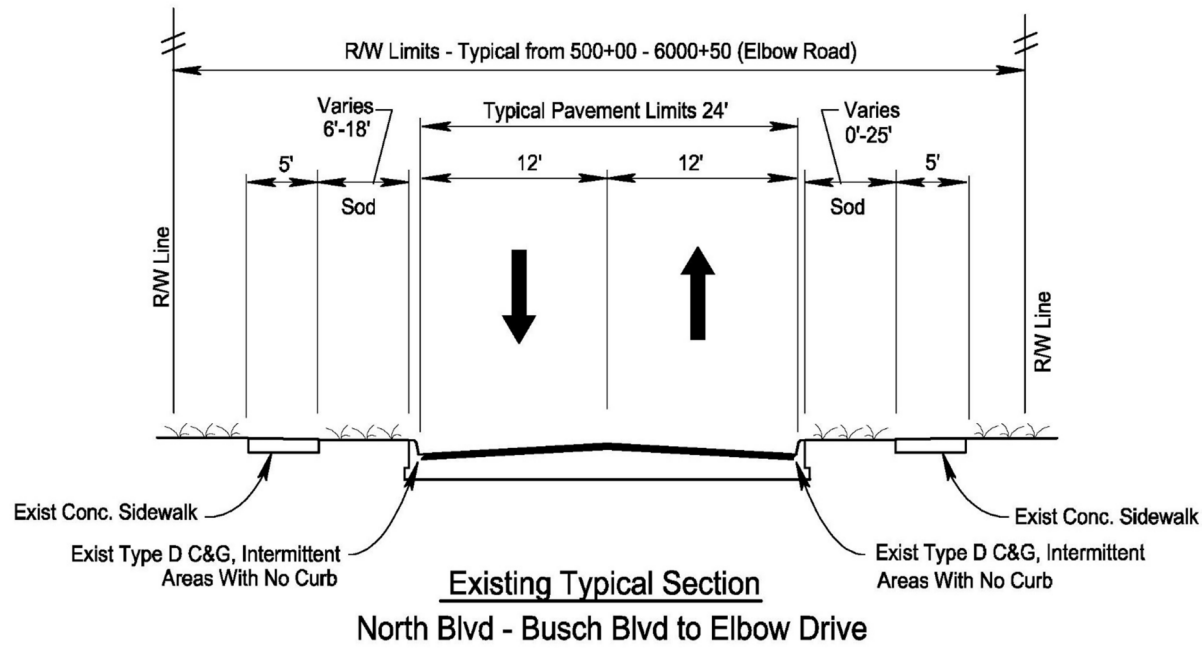


City of
Tampa
Florida

DRAFT

Prepared:
October 2019
By S&ME, Inc.





Utility corridor east side



Large green/swale area, west side

Type D C&G (configuration varies)

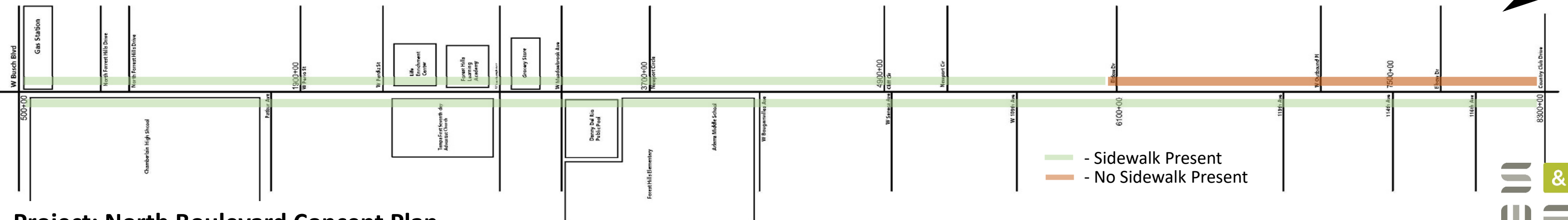
Existing Typical Section

North Boulevard from Busch Boulevard to Elbow Drive is a two-lane undivided roadway with 12' lanes and predominantly 5' sidewalks on both sides of the roadway. The section of North Boulevard from Elbow Drive to Country Club Road is also two-lane divided with 12' lanes, but only has a 5' sidewalk on the east side of the roadway. Both sections include predominantly sodded areas between the sidewalk and edge of pavement or back of curb. There is curb and gutter along large sections of the roadway, however most of the curbing is a Type D curbing that varies in face reveal height and curb width. Some areas, mostly at intersections, have had Type F curb and gutter installed probably to help with drainage.



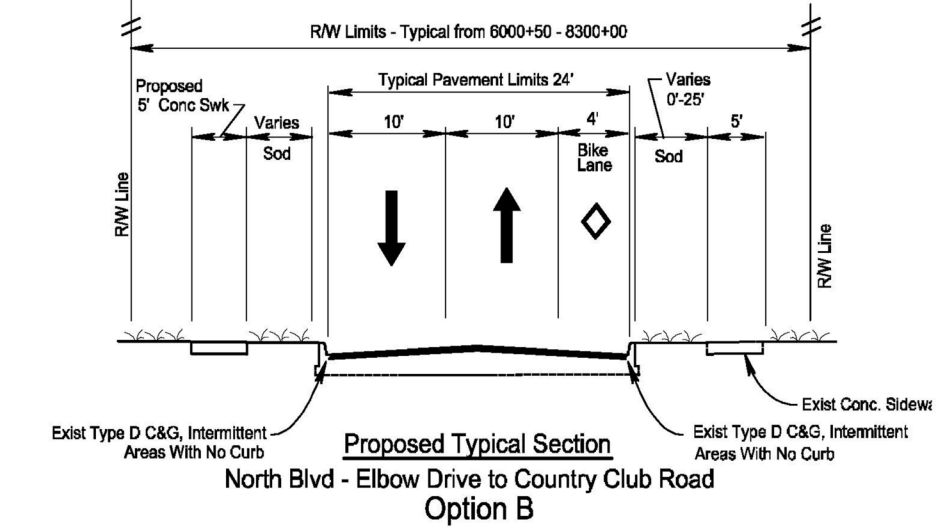
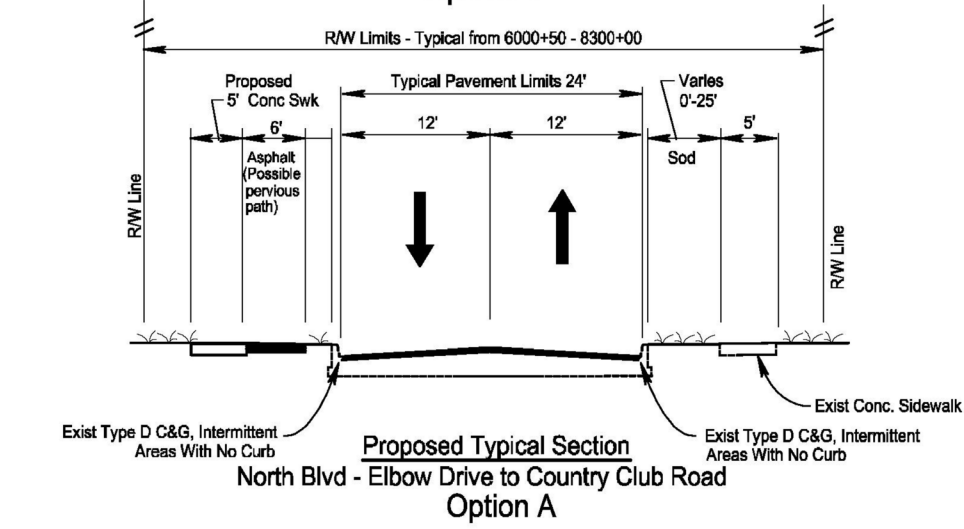
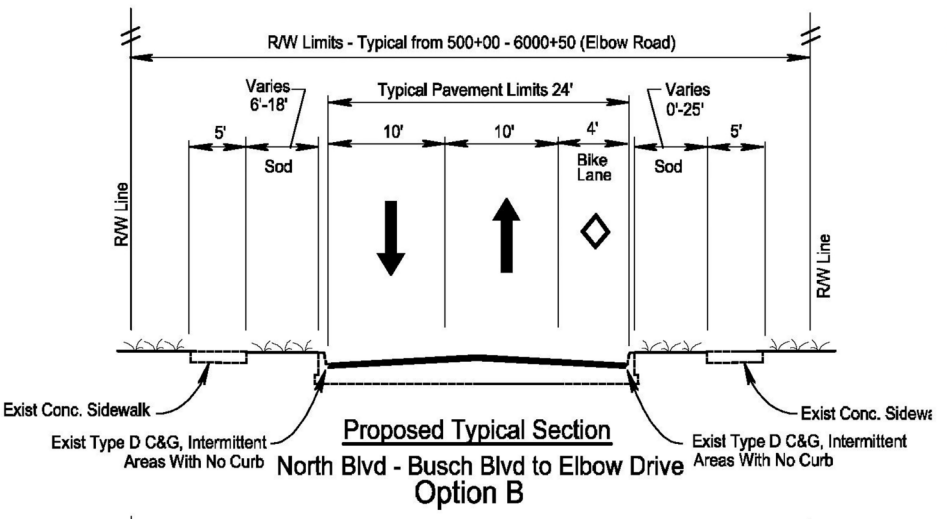
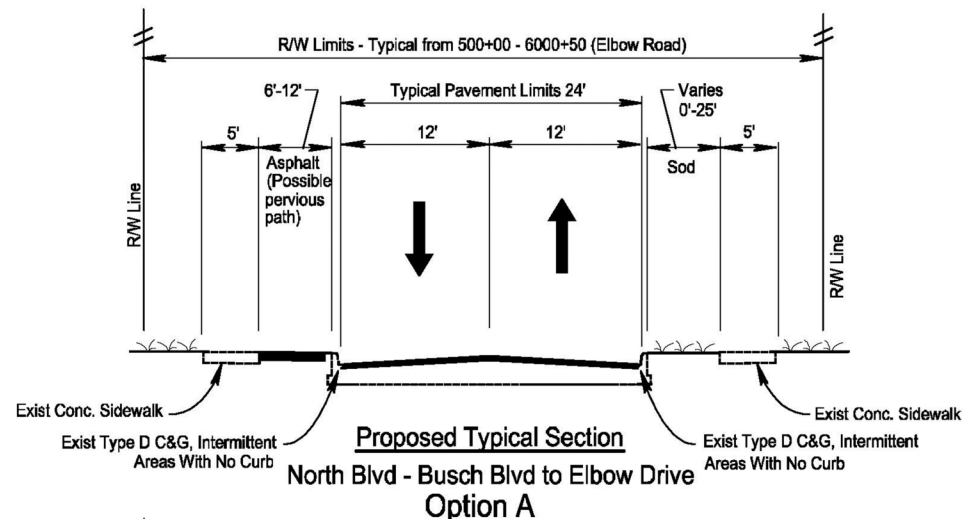
Type D C&G (configuration varies)

Key Map



Proposed Typical Section Options

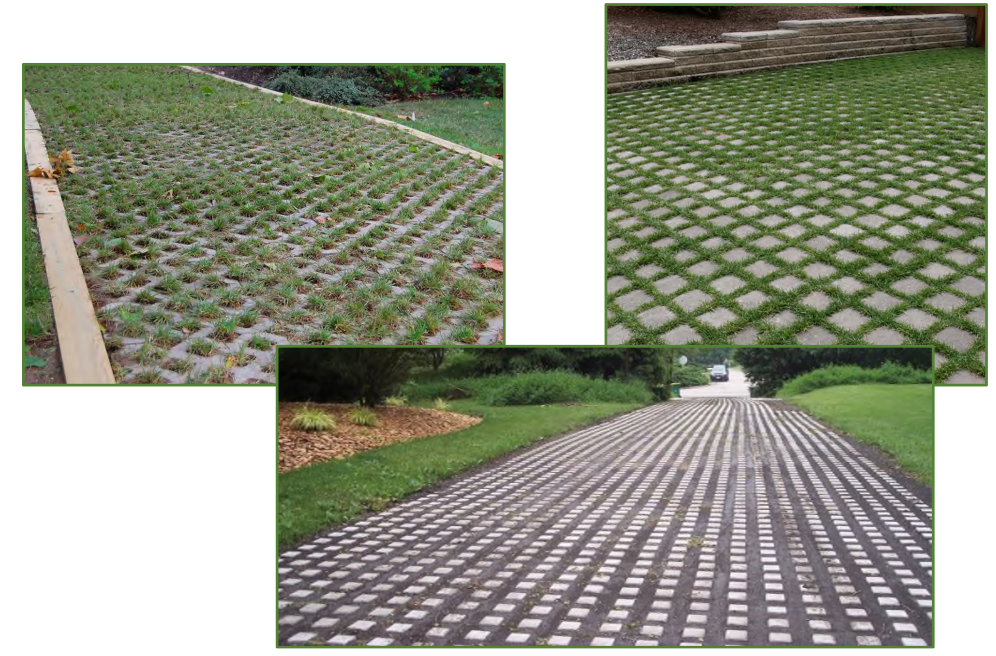
Proposed Typical Section Options



Option A
Would install a 6'-12' trail for bikes alongside the existing 5' sidewalk on the west side of the roadway. The east side of the roadway includes a large service utility corridor with large metal poles and infrastructure. The west of the road has fewer obstacles to construction. Based on the three public schools in the corridor, walking and biking infrastructure is important. There is the option to use permeable materials for the trail since the 5' sidewalk would still be ADA compliant.

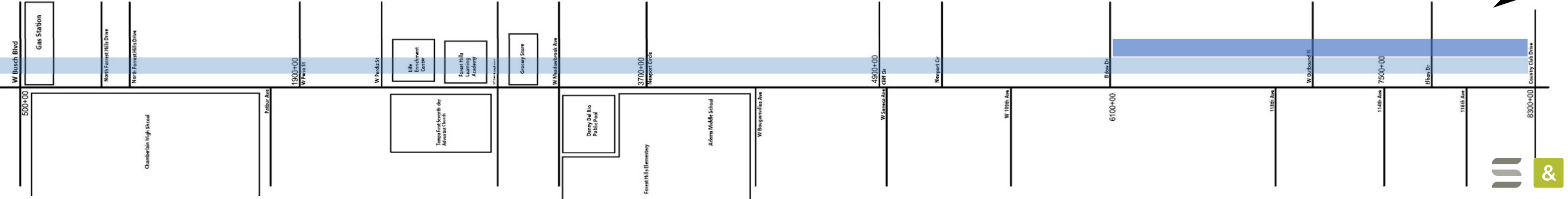
Option B
Would reduce the thru lane widths from 12' to 10' and add one 4' bike lane on the east side of the roadway. The three public schools in the corridor are all on the east side of the corridor, therefore, there would be fewer crossings of the roadway to access the bike lane. This is not an optimal configuration, and if funds were available, then 4' of widening on the west side of the roadway to include another bike lane would be a better solution.

- If the funds were available, then rebuilding this section of North Boulevard with a new profile, Type F curb & gutter, upgraded drainage system, and widening into the areas between the sidewalks and curb, then full bike lanes on both sides would be the clear option. Leaving the majority of the roadway 'as-is' limits the options available for implementation.
- Option A adds a bike trail alongside the sidewalk on the west side of the roadway. It is also recommended that the 5' sidewalk be continued on the west side from Elbow Drive to the Country Club Road.
- Option B would narrow the lanes from 12' to 10' and add one 4' bike lane. It is also recommended that the 5' sidewalk be continued on the west side from Elbow Drive to the Country Club Road.



Option A permeable trail materials.

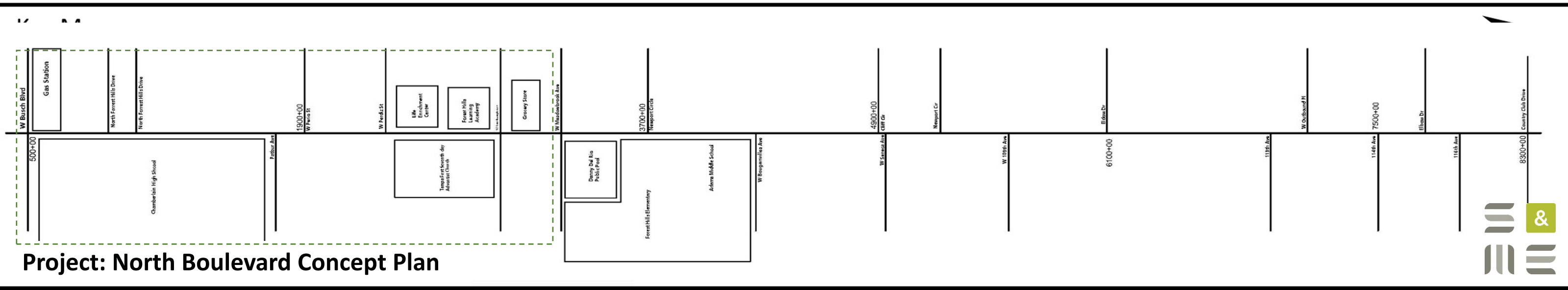
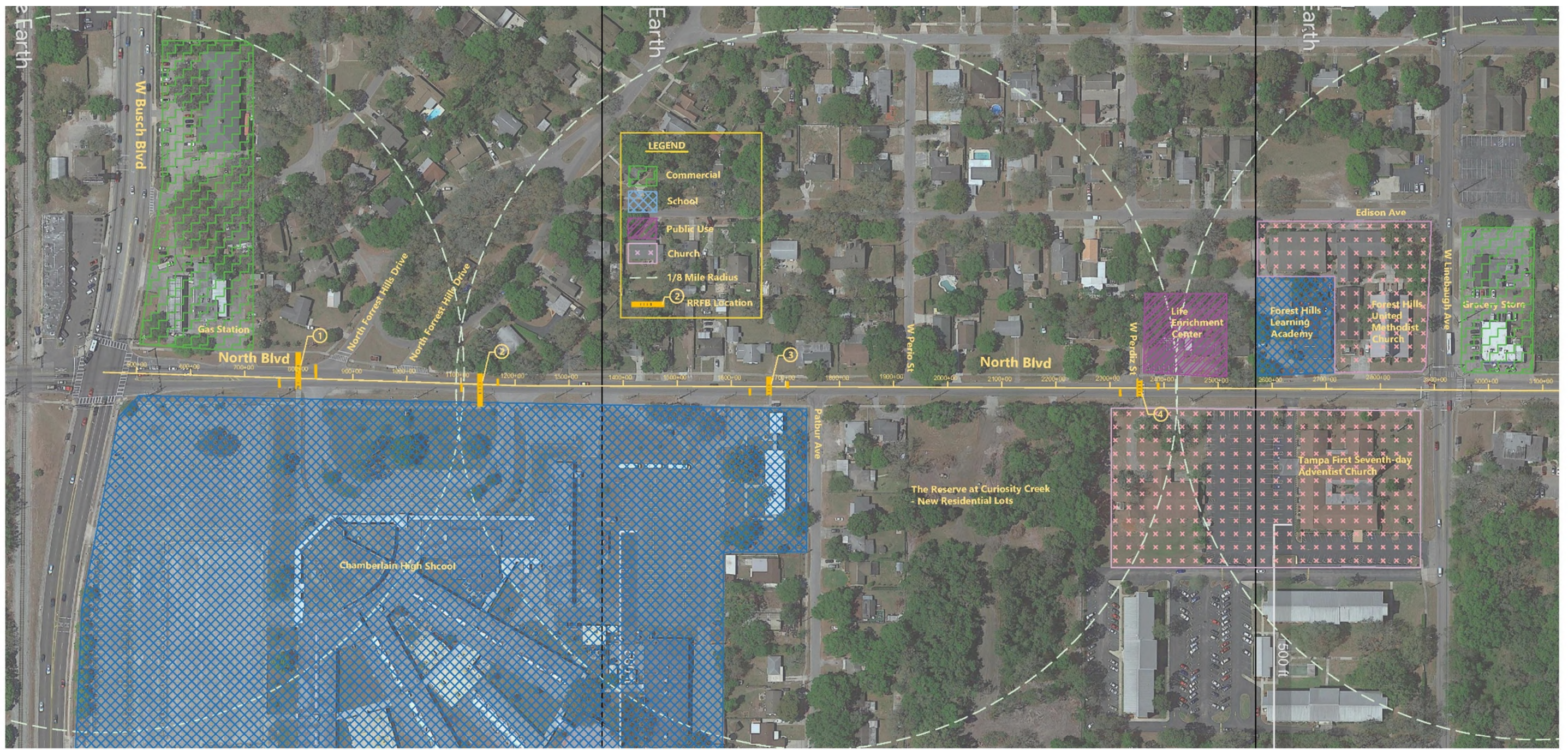
Key Map



Project: North Boulevard Concept Plan

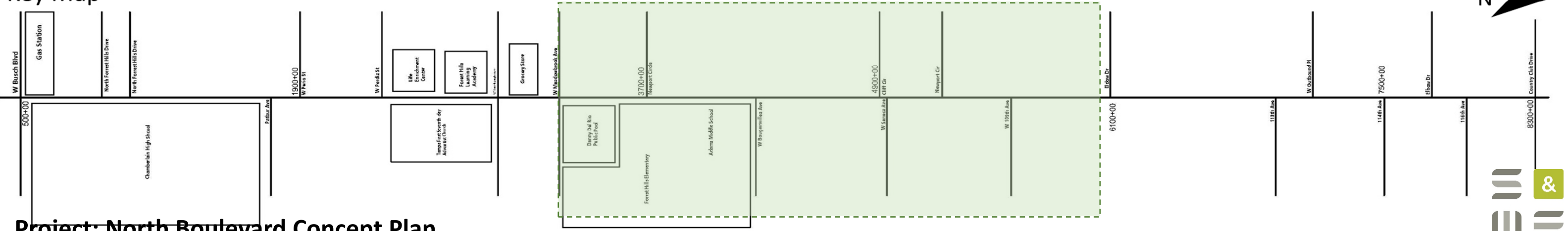
- Proposed Bike Trail (Option A)
- Proposed 5' Sidewalk (Option A and Option B)







Key Map

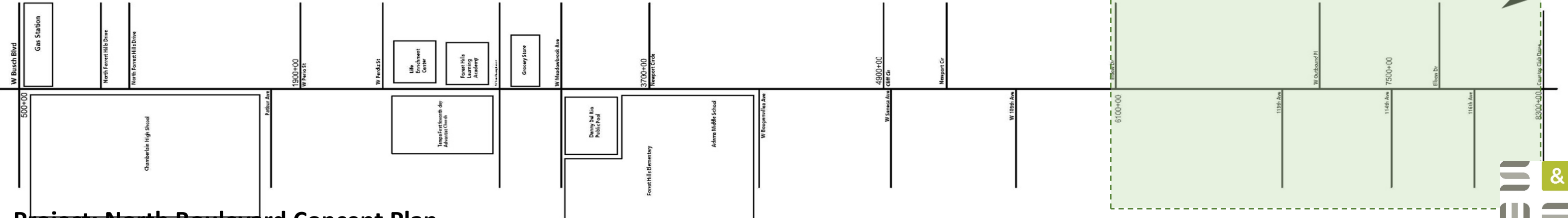


Project: North Boulevard Concept Plan





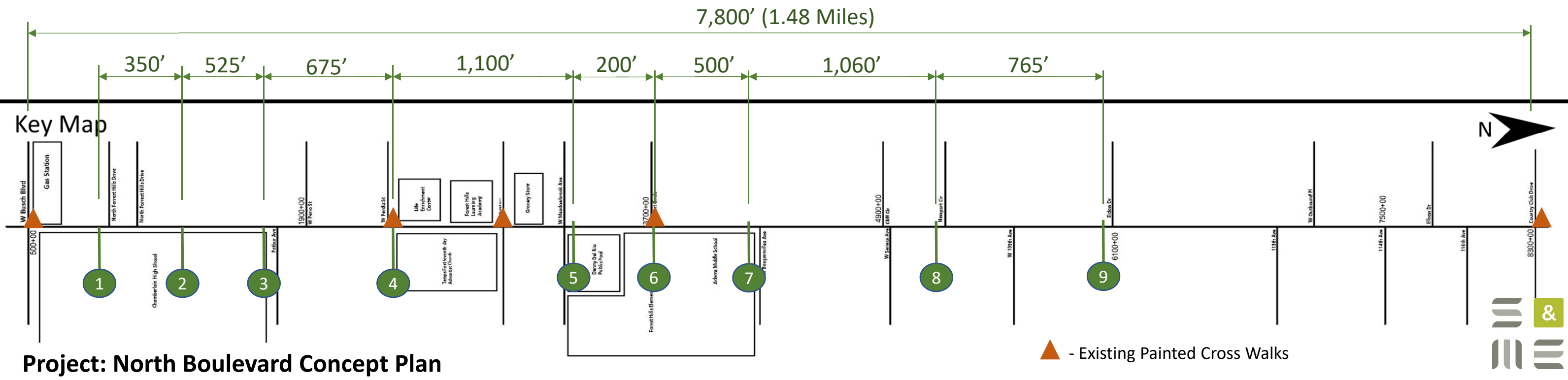
Key Map



Project: North Boulevard Concept Plan

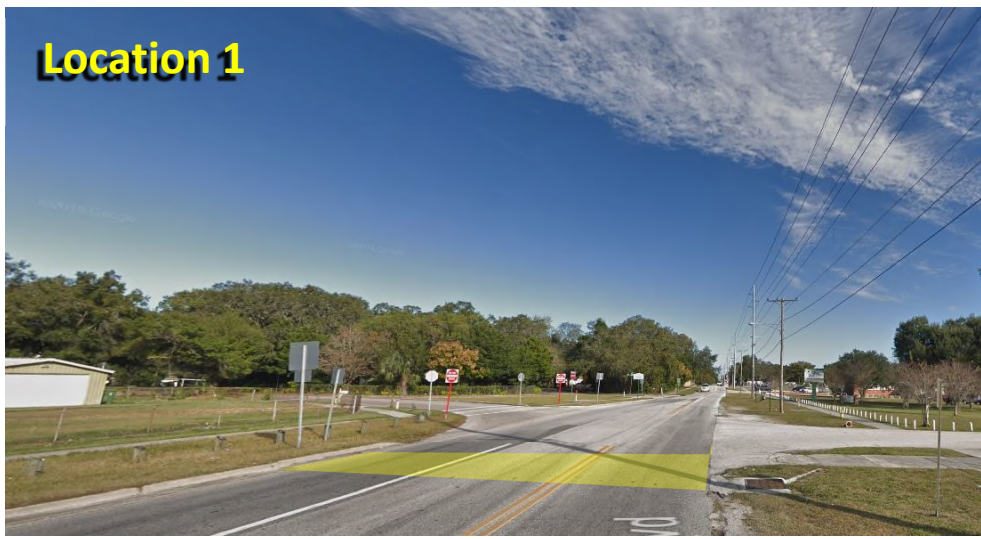


Number	Station	Location Description	Design Considerations
1	800+00	South of North Forrest Hills Drive	Connects to school sidewalk. Provides access to commercial.
2	1100+50	North of North Forrest Hills Drive	Connects to school sidewalk. Adjacent drive way is right-out only, away from the cross walk.
3	1600+75	South of Patbur Avenue	Existing bus pad, connects the east and west side neighborhoods.
4	2300+50	North of Perdiz Street	Connects to Life Enrichment Center, A senior and disability leisure and education center. Existing cross walk location.
5	3400+50	North of W Meadowbrook Avenue	At south end of school zone for an elementary and middle school, also serves Danny Del Rio Public Pool.
6	3600+50	South of Newport Circle	Inside of school zone for an elementary and middle school. Provides crossing north of school and pool driveways. Existing cross walk location.
7	4100+50	South of W Bougainvillea Avenue	Connection to existing bus stop with bus shelter.
8	5200+10	South of W 109th Avenue	Neighborhood connection. Existing bus pad.
9	5900+75	South of Elbow Drive	On the north end of the corridor, last crossing opportunity before sidewalk on west side ends.

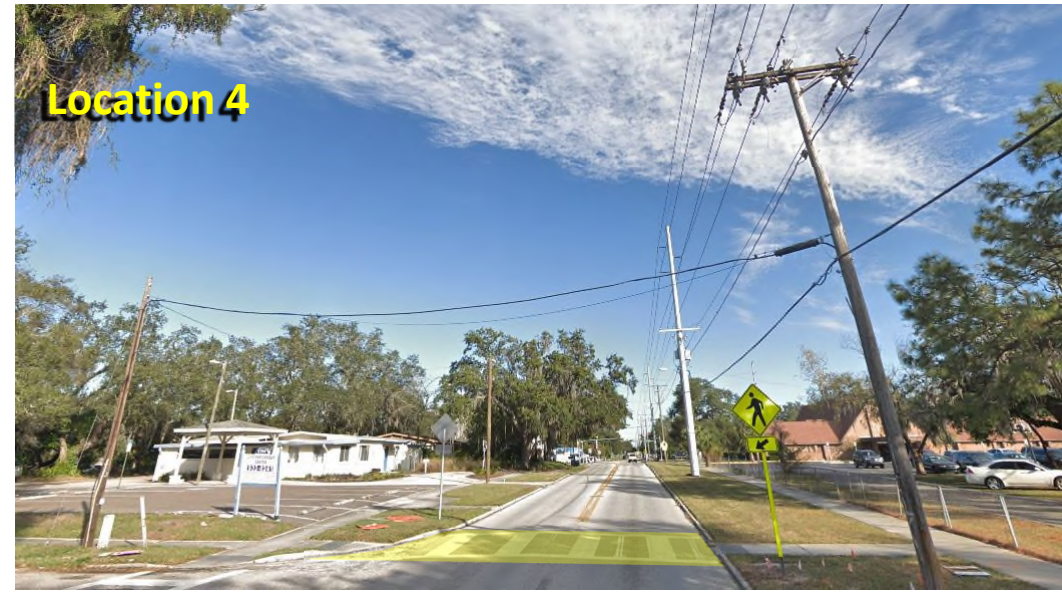


Note: All pictures are facing north.

Location 1



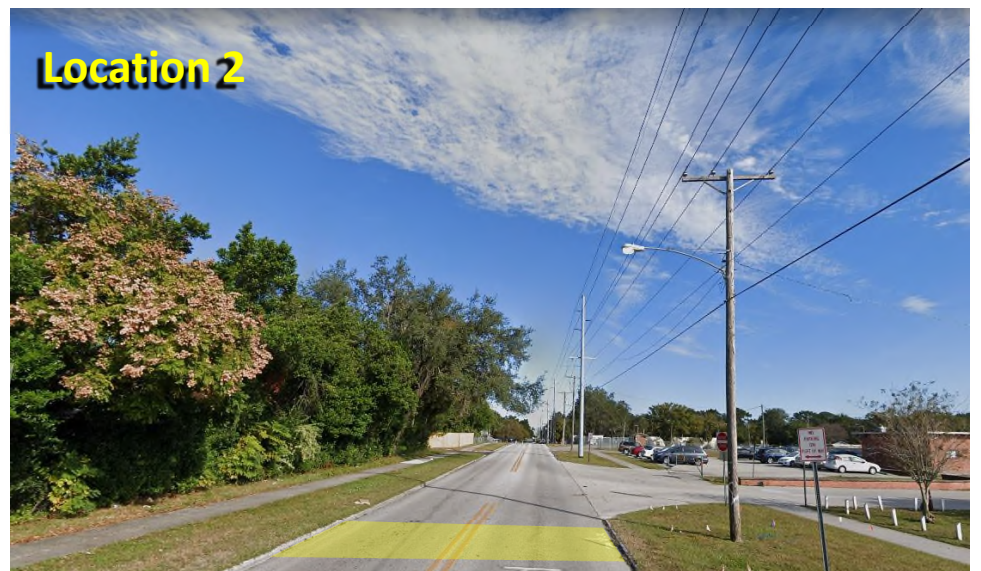
Location 4



Location 7



Location 2



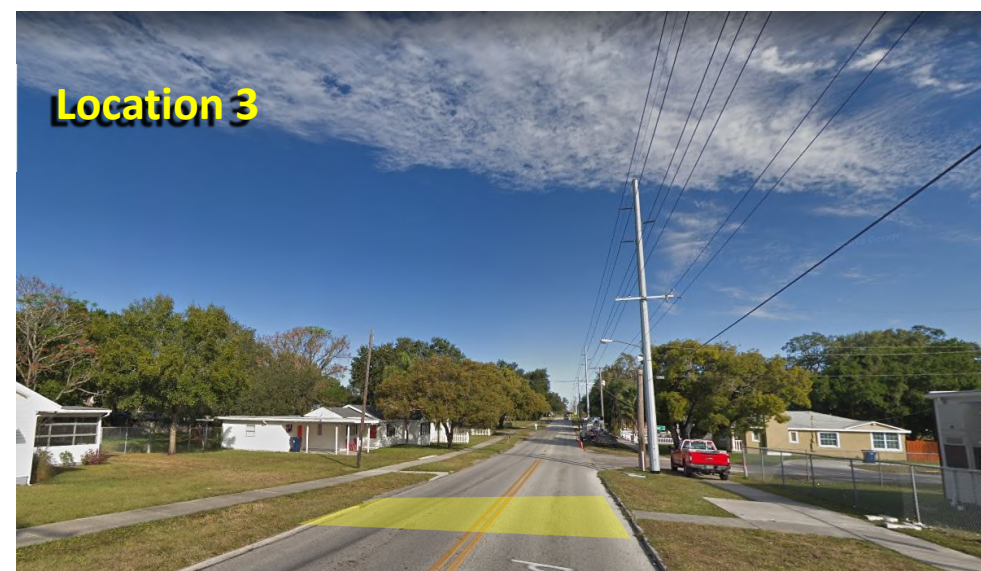
Location 5



Location 8



Location 3



Location 6



Location 9





City of Tampa

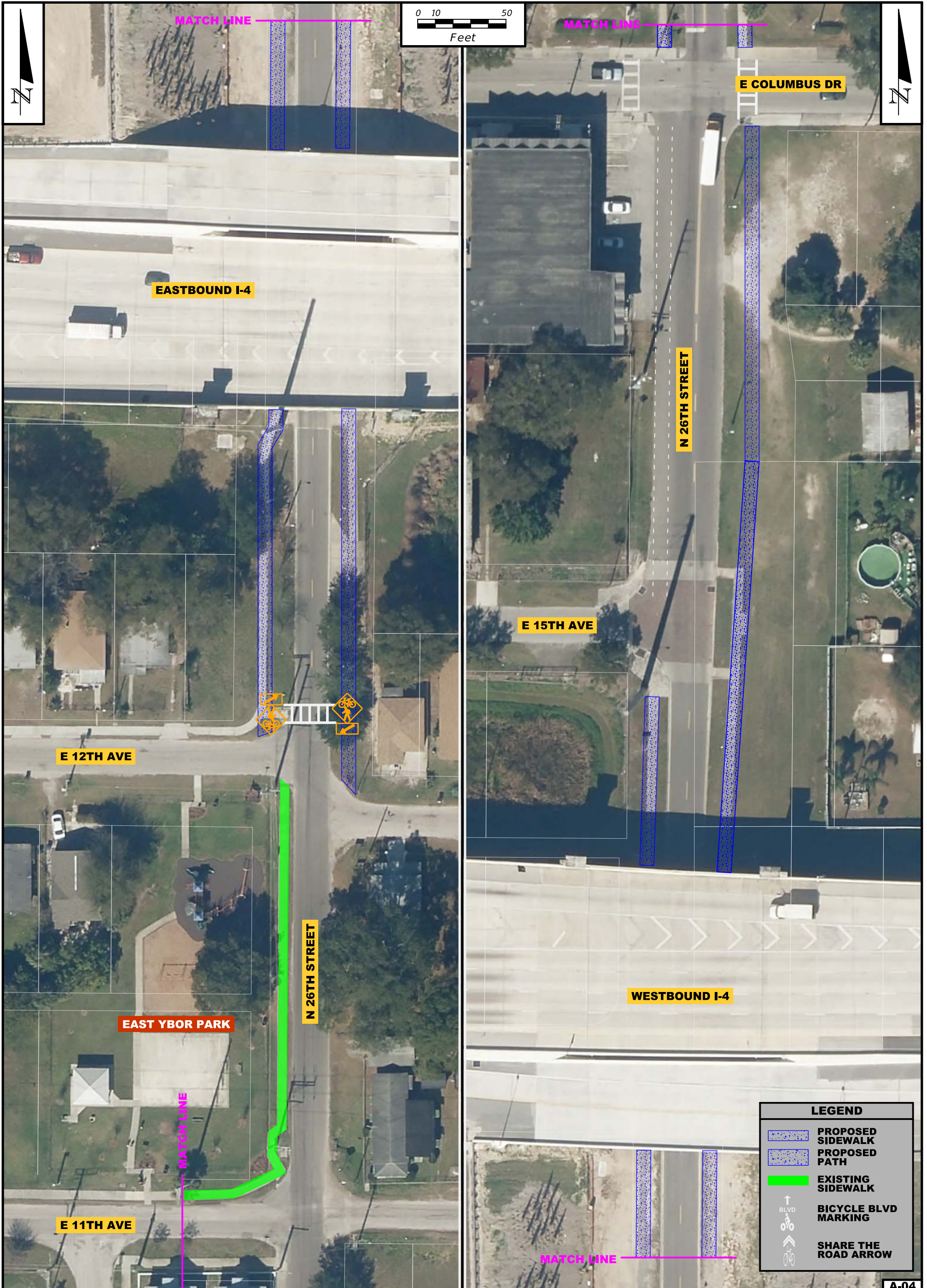
Walk-Bike Plan Phase V

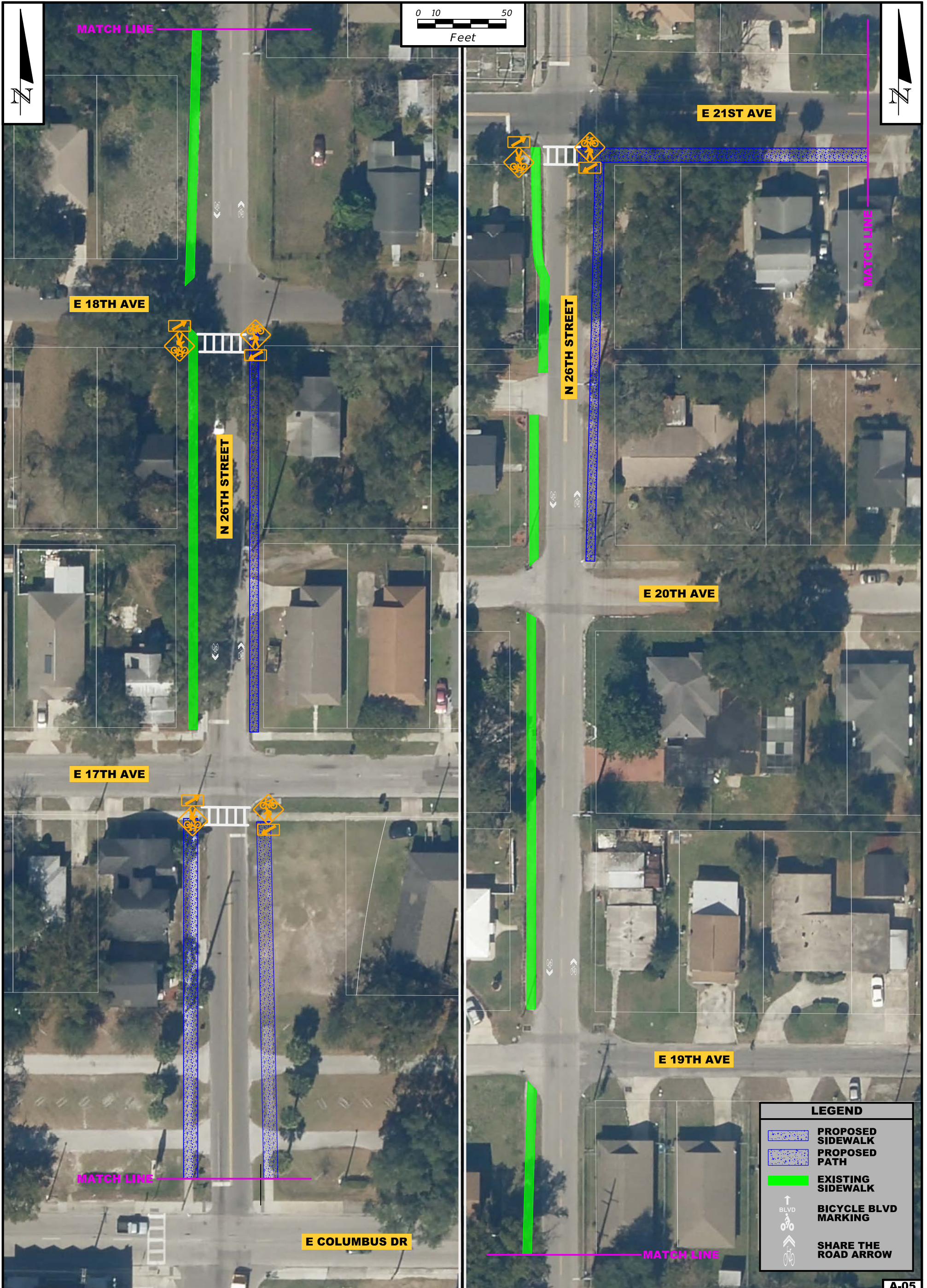
December 2015








Hillsborough MPO
Metropolitan Planning
for Transportation

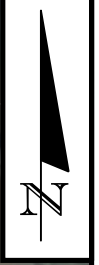






LEGEND

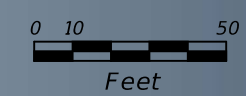
-  PROPOSED SIDEWALK
-  PROPOSED PATH
-  EXISTING SIDEWALK
-  BICYCLE BLVD MARKING
-  SHARE THE ROAD ARROW



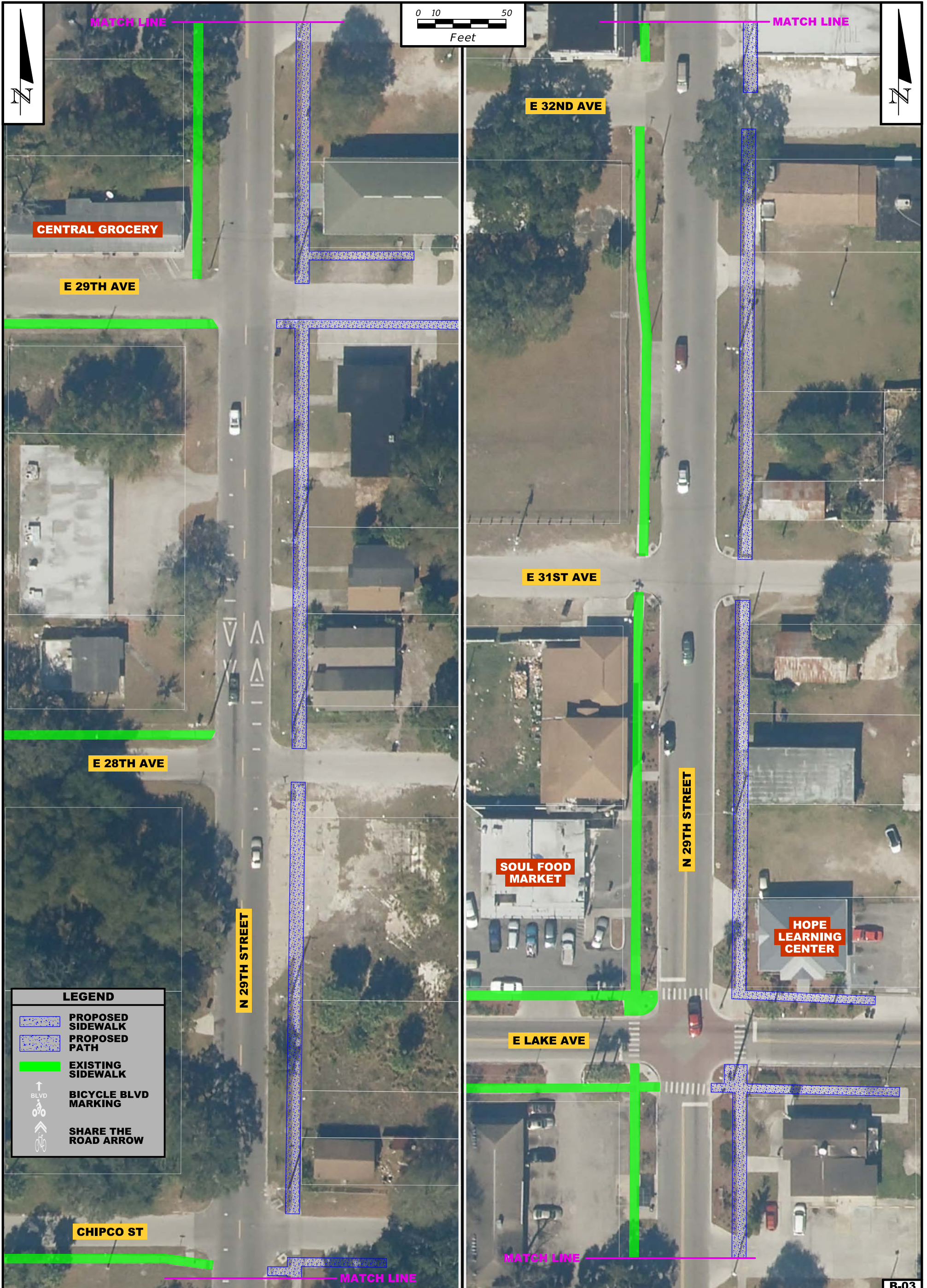
B-01

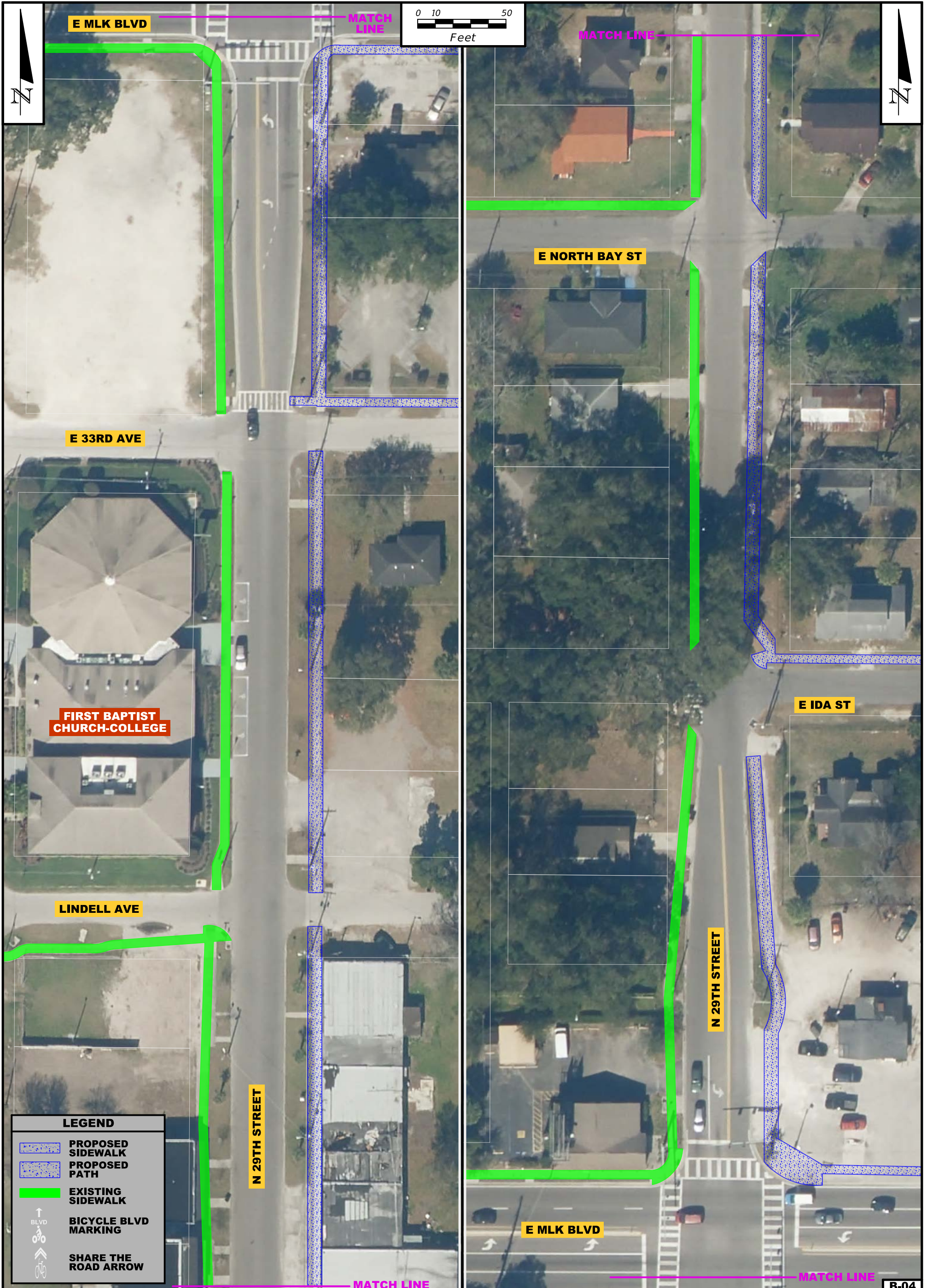
ATKINS

Walk-Bike Plan Phase V



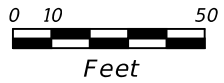






E MLK BLVD

MATCH LINE



MATCH LINE

E 33RD AVE

E NORTH BAY ST

FIRST BAPTIST CHURCH-COLLEGE

E IDA ST

LINDELL AVE






N 29TH STREET

E MLK BLVD

MATCH LINE

B-04

LEGEND

-  PROPOSED SIDEWALK
-  PROPOSED PATH
-  EXISTING SIDEWALK
-  BICYCLE BLVD MARKING
-  SHARE THE ROAD ARROW

ATKINS

Walk-Bike Plan Phase V

