

Floribraska Ave. Complete Streets

City of Tampa
Department of Mobility
Virtual Public Meeting
February 21, 2023

Title VI of the Civil Rights Act of 1964

No person shall, on the basis of his or her race, color, or national origin, be excluded from participation in, be denied the benefits of, or be subjected to discrimination under any program or activity receiving Federal financial assistance pursuant to the requirements of Title VI of the 1964 Civil Rights Act.

The Mayor and City Council value diversity and welcome input from all interested parties. Moreover, the City does not tolerate discrimination in any of its federally assisted programs, services or activities. The City of Tampa will not exclude participation in, deny the benefits of, or subject to discrimination anyone on the grounds of race, color, and national origin.

File a Complaint

Any person who believes that he or she has been subjected to discrimination based upon race, color, and national origin, may file a complaint with the City's Title VI Officer:

Kayon Henderson

Housing and Community Development Manager

City of Tampa / 4900 W. Lemon St. / Tampa, FL 33609

p: (813)274-7999/ f: (813)274-7941/ e: kayon.henderson@tampagov.net

Please Visit us on the web at: <https://www.tampagov.net/planning-and-development/human-rights> for instructions on how to properly file a complaint.

Appeal a Decision

Any person who decides to appeal any decision(s), made with respect to any matter considered at this meeting, is advised that they will need a record of the proceedings. For such a purpose, they may need to hire a court reporter to ensure that a verbatim record of the proceedings is made, which includes the testimony and evidence upon which the appeal is to be based.

Mayor Castor's T3 Initiative

Mayor Jane Castor

Transforming Tampa's Tomorrow



1. Implement Strategic Transit Projects.
2. Reimagine Trails and Greenways as Viable Transportation Options.
3. Adopt Vision Zero as a Citywide Policy.
4. Reinvent Urban Parking and Mobility.
5. Enhance Neighborhood Engagement.

VISION
ZERO
TAMPA



PROJECT TEAM



Project Team



Nina Mabileau, E.I.
Project Manager



Cal Hardie, P.E.
Chief Design Engineer
Engineer of Record



Lara Bouck, P.E., AICP
Chief Production Engineer

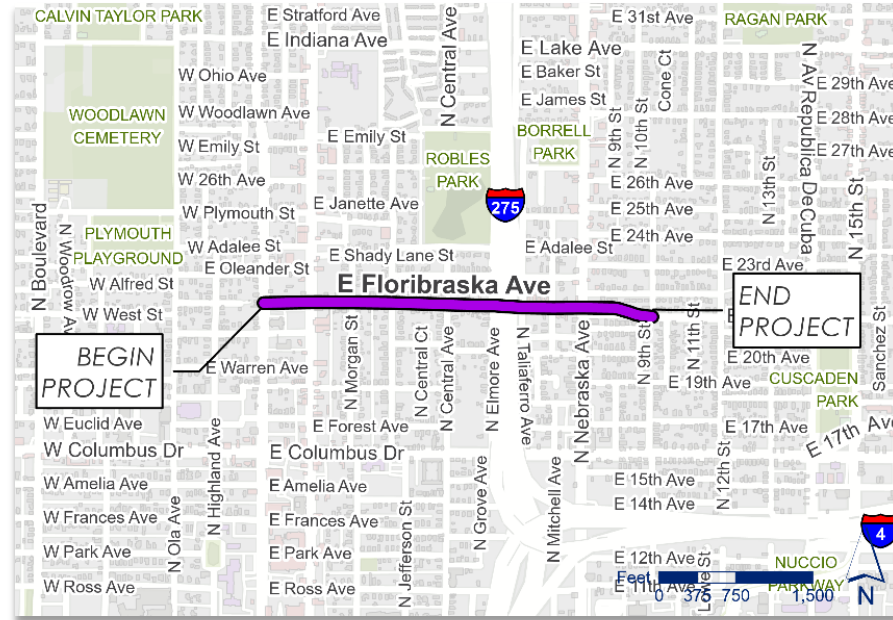
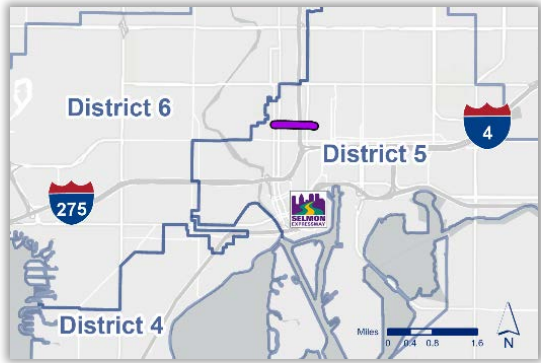


Signal & Drainage Design and Utility
Coordination



PROJECT OVERVIEW

Floribraska Ave Complete Streets



PROJECT DESCRIPTION

This project provides for reconfiguring the existing 4-lane undivided roadway to a complete street with two (2) 11-foot travel lanes, a two-way left turn center lane plus an on-road bicycle lanes in each direction and enhanced crosswalks at the intersections of both Jefferson St. and Central Avenue.

BENEFITS TO THE COMMUNITY

The proposed improvements will reduce conflicts and sight obstruction hazards along the corridor making it safer for all users in accordance with the City's Vision Zero initiative while maintaining the existing roadway's level of service. Improved bicycle connectivity with proposed dedicated on-road bicycle lanes with pavement messaging.

PROJECT COST TOTAL EST.: \$2,047,562

ESTIMATED SCHEDULE

Construction Year: 2023

Projected Start Date: June 2023

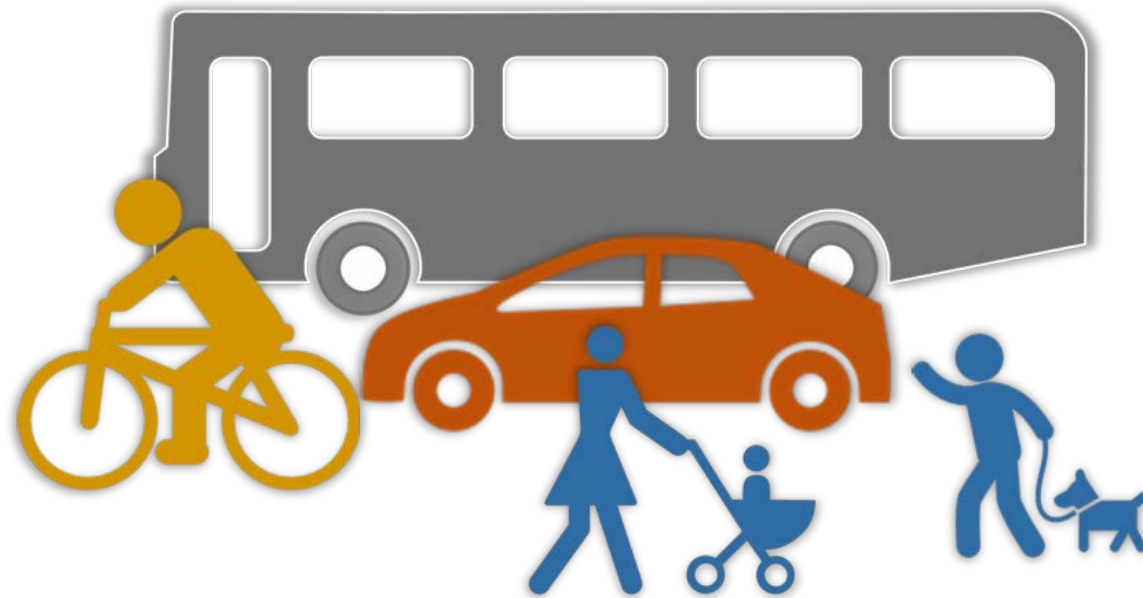
Projected End Date: August 2024

Funding Source: LOGT & FDOT LAP Grant

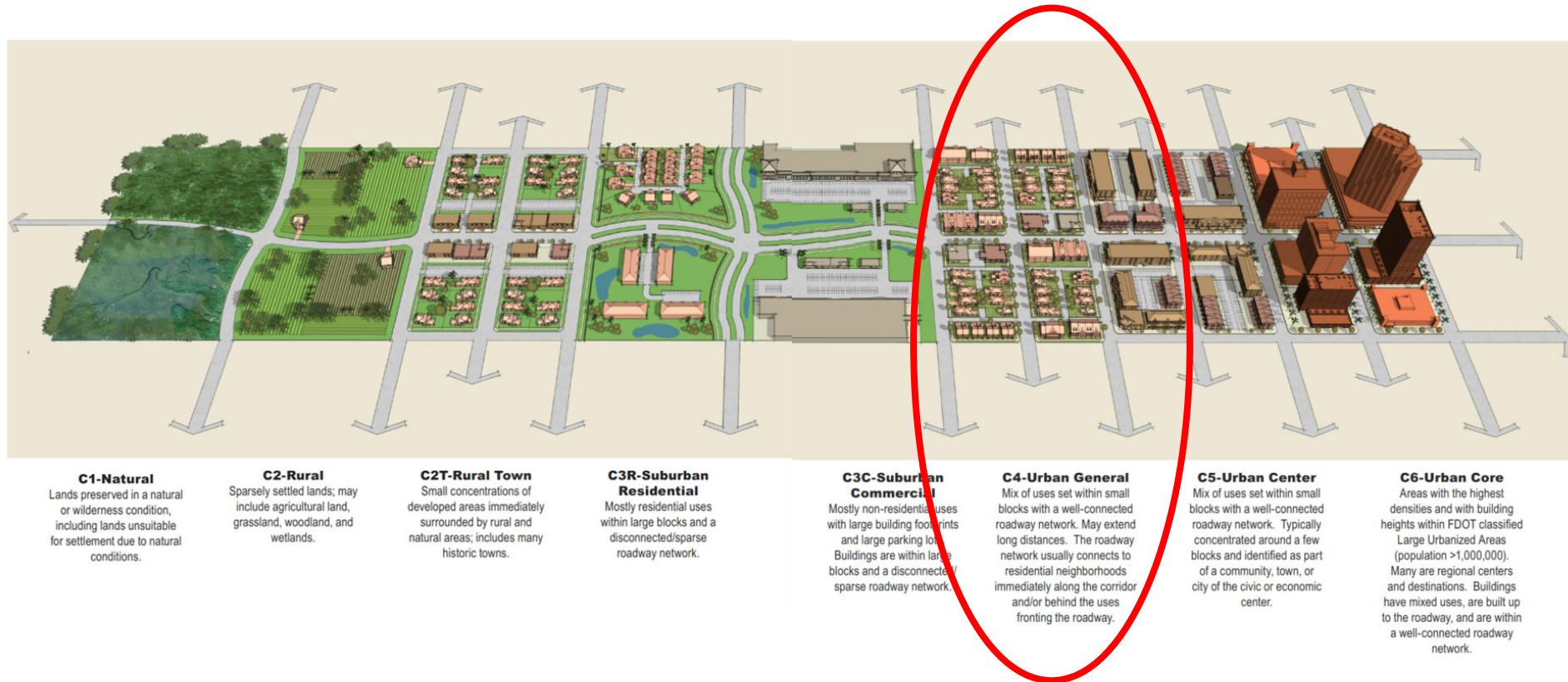
PROJECT BACKGROUND

What are Complete Streets?

Complete Streets are designed so all modes of transportation can share the road safely.



The Right Street in the Right Place



There is no single design application for Complete Streets; each one is unique and responds to its community context.

What is Floribraska Avenue?



Arterial Roadway

- East-West Connector to several neighborhoods
- Connection to I-275



Commercial Corridor

- Retail Stores
- Service Businesses
- Restaurants
- Corner Stores
- Coffee Shops



Transit Route

- HART Route 6
- 4 Bus Stops within Project Limits



Neighborhood Center

- Multiple Places of Worship
- Crossings for Parks and Schools



Residential Roadway

- Residences line both sides of the street

Floribraska Avenue

WHAT IS A “ROAD DIET?”

Safety- Overview

- A Federal Highway Administration (FHWA) **Proven Safety Countermeasure**
- Benefits include:
 - Safety
 - Ease of Use
 - Provision for Bike Lanes
 - Better Pedestrian Experience



Road Diet Candidates

LESS THAN 10,000 ADT

Great candidate for Road Diets in most instances. Capacity will most likely not be affected.



10,000 – 15,000 ADT

Good candidate for Road Diets in many instances. Agencies should conduct intersection analysis and consider signal retiming to determine any effect on capacity.

15,000 – 20,000 ADT

Good candidate for Road Diets in some instances. Agencies should conduct a corridor analysis. Capacity may be affected at this volume depending on the "before" condition.

GREATER THAN 20,000 ADT

Agencies should complete a feasibility study to determine whether this is a good location for a Road Diet. There are several examples across the country where Road Diets have been successful with ADTs as high as 26,000. Capacity may be affected at this volume.

1 FHWA, Road Diet Informational Guide, FHWA-SA-14-028 (Washington, DC: FHWA, 2014). Available at: http://safety.fhwa.dot.gov/road_diets/case_studies/roaddiet_cs.pdf.

2 City of Seattle Modeling Flow Chart for Road Diet Feasibility Determination. Available at: http://safety.fhwa.dot.gov/road_diets/info_guide/ch3.cfm#f1.

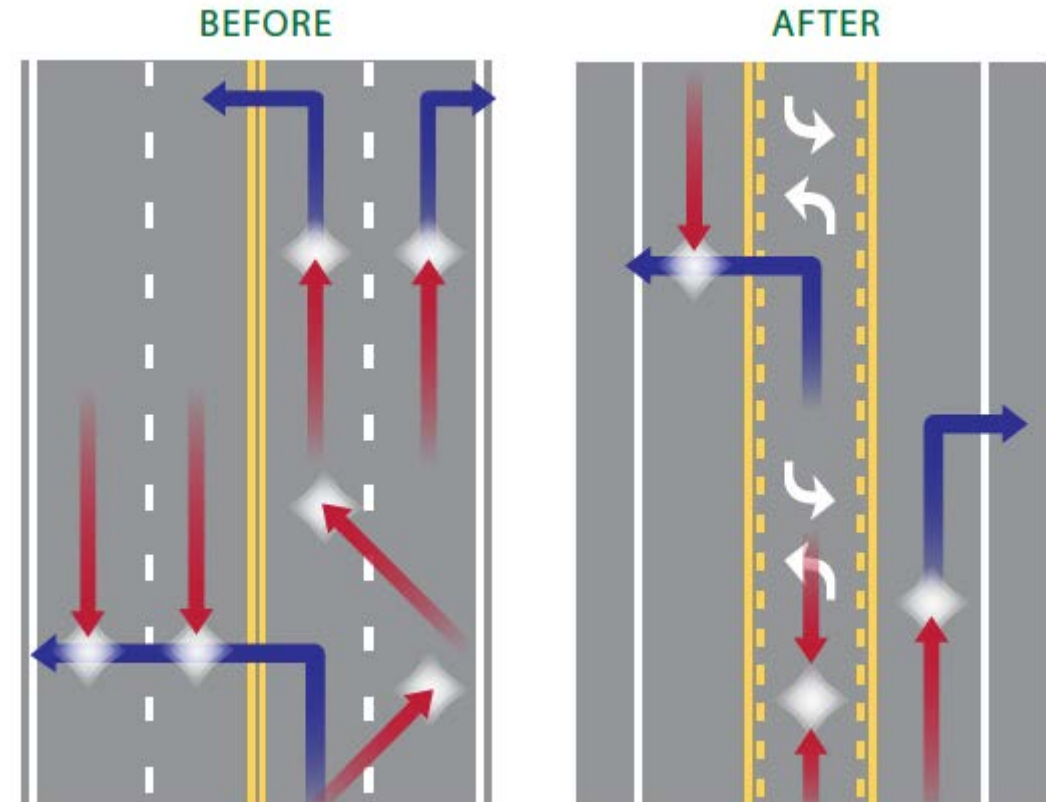
3 MnDOT Office of Traffic, Safety and Technology, Minnesota's Best Practices for Pedestrian/Bicycle Safety, Report 2013-22 (Roseville, MN: MNDOT, 2013). Available at: <http://www.dot.state.mn.us/stateaid/trafficsafety/reference/ped-bike-handbook-09.18.2013-v1.pdf>.

Floribraska Ave (Florida Ave. to Nebraska Ave.) AADT= 7,712

Hillsborough Metropolitan Planning Organization Traffic Counts, 2018

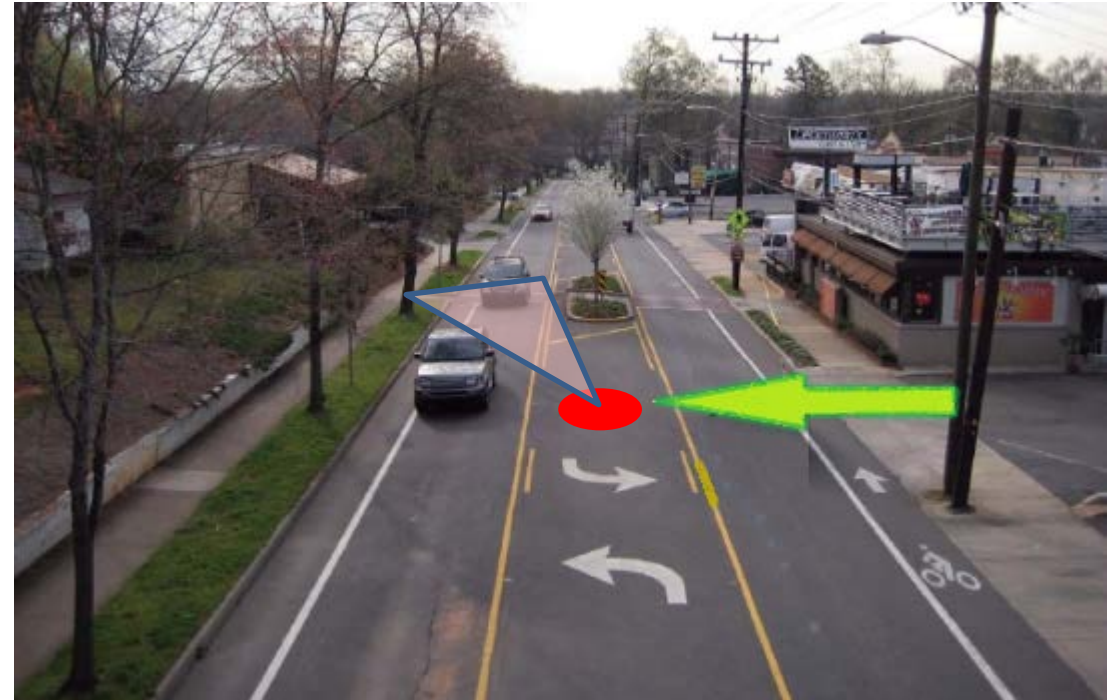
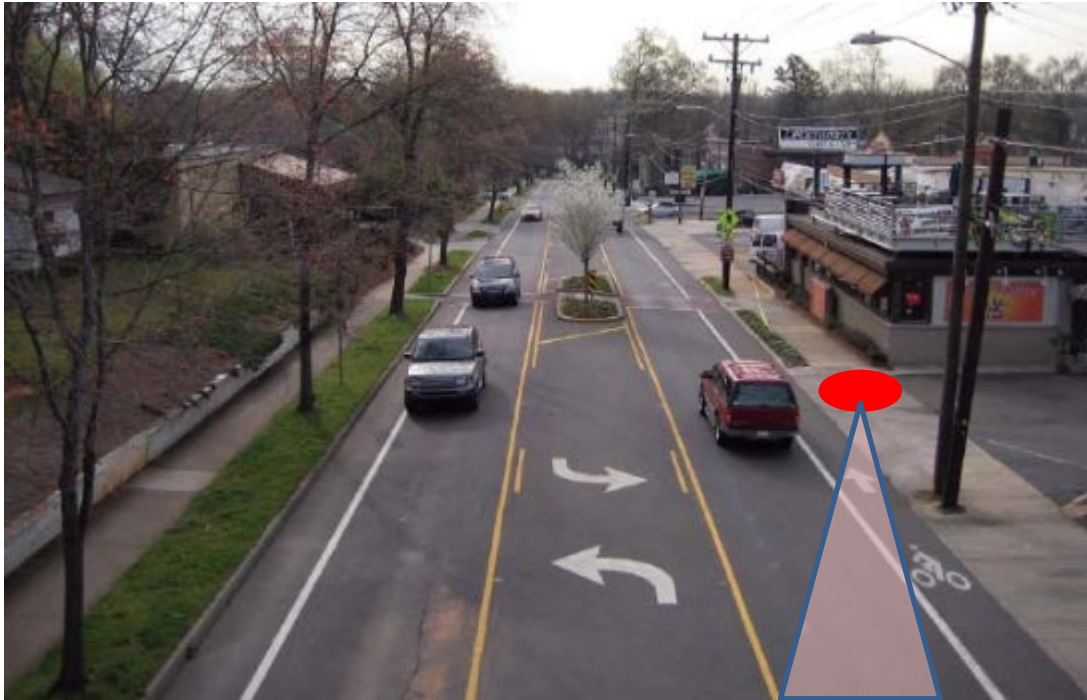
Safety- Crash Statistics

- Road Diets typically reduce total crashes **19% to 47%**
 - Knapp, Keith et al. (November 2014). *Road Diet Informational Guide (FHWA-SA-14-028)*
- **29% reduction in total crashes**
 - 15 Case Studies in Iowa, 30 sites in California and Washington
 - 7-15% Increase in Traffic
 - *Evaluation of Lane Reduction "Road Diet" Measures on Crashes, Publication Number: FHWA-HRT-10-053, June 2010, FHWA*



Conflict Points are areas where two vehicles cross paths. These areas are opportunities for crashes. The two pictures above show the reduction in conflict points associated with a road diet.

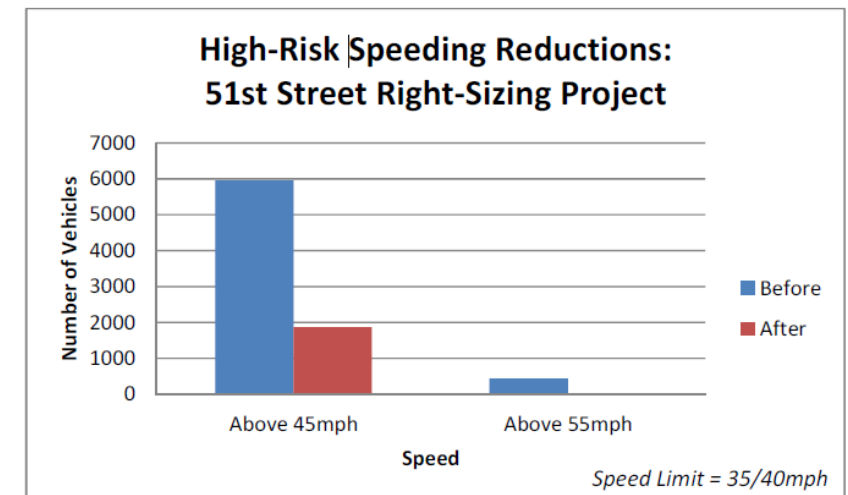
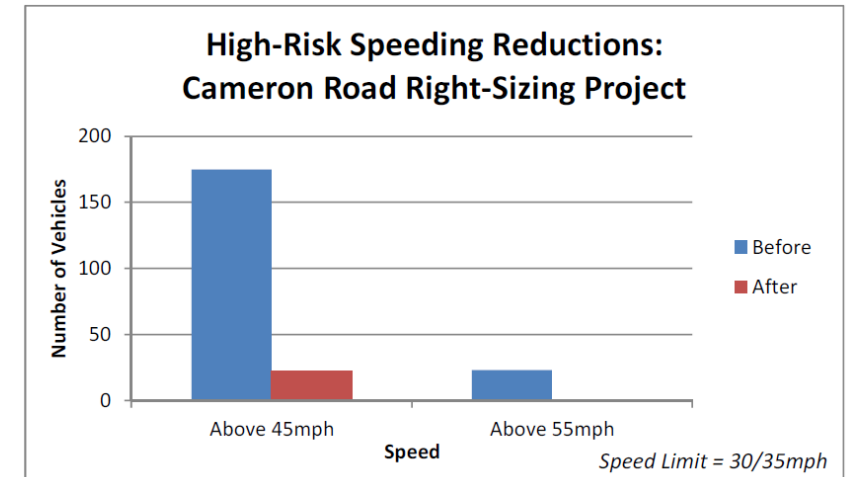
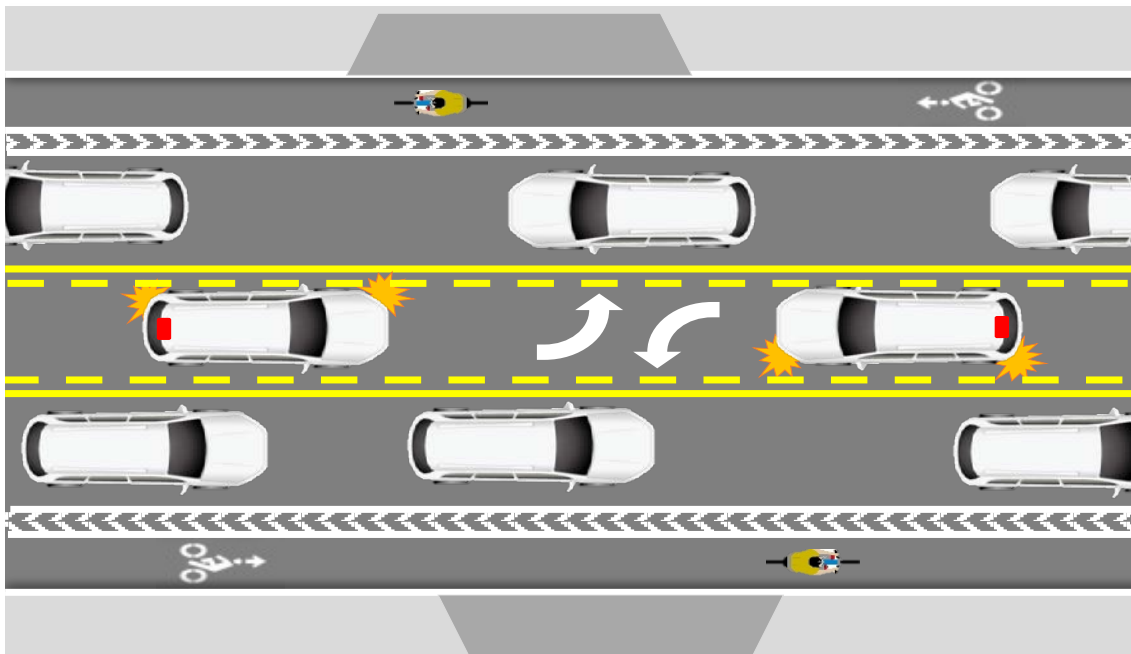
Left Turns from Side Streets and Driveways



Left turns from side streets and driveways are safer and easier because a driver only crosses one lane at a time. The two-way left turn lane provides an area for a driver to pause and check oncoming traffic in the other direction.

Lower Speeds (No Passing)

- Slower Vehicular Speeds
 - One travel lane allows for effective speed controls as there is no passing lane



Source: *Redesigning the Street, 2014, City of Austin Texas*

RIGHT-SIZING THE STREET

What is Right-Sizing?

- Generally, as the City grew, roads were traditionally widened to add additional through lanes as that was the known practice at the time.
- Floribraska Ave. has excess space dedicated to vehicles that sits largely unused.
- Right-sizing is the process of reallocating pavement and right-of-way space to better serve the context of the roadway and goals of the community

Pedestrian Safety



Figure 1: Speed is the number one contributing factor to pedestrian deaths

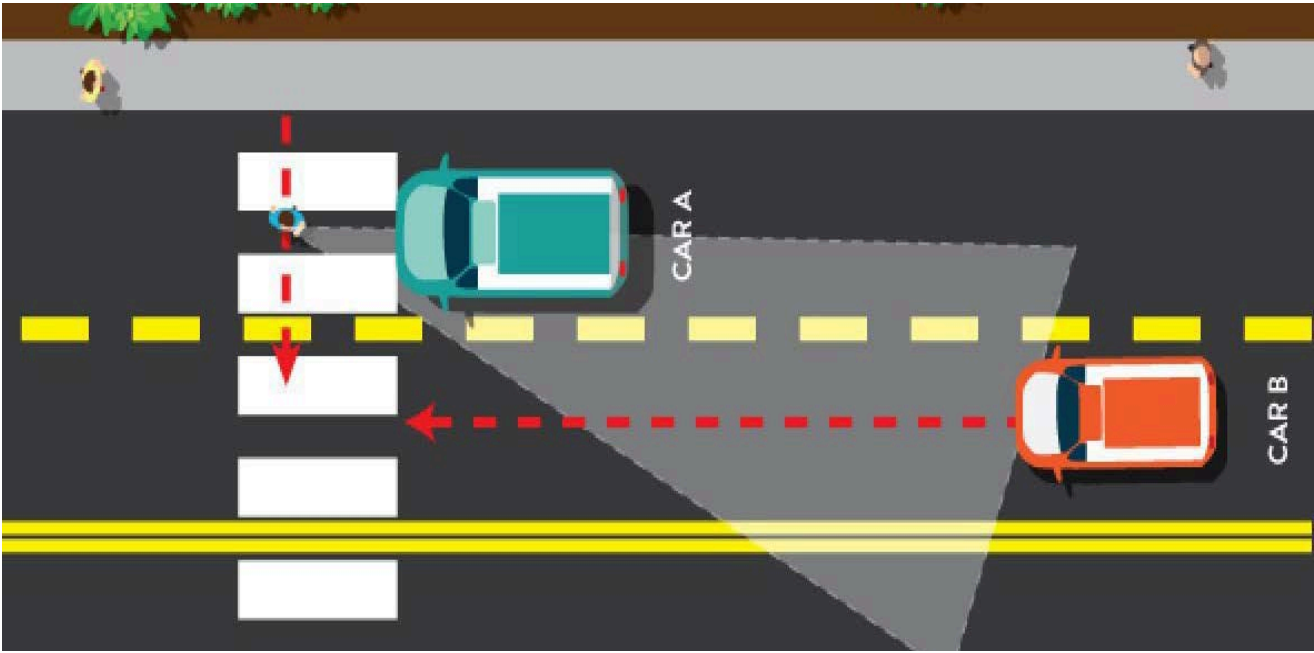


Figure 2: Removal of travel lanes eliminates the risk of Multiple Threat Crash with pedestrian/vehicle obscured.

PROJECT UPDATES

Recent Developments

- FHWA requires all projects adjacent to an interstate highway interchange to undergo an Interchange Operational Analysis Report (IOAR)
- At final plan submittal, November 2022, the City was informed of this process
- The IOAR has required significant design revisions.

Previous Design

E FLORIBRASKA AVE



Right-sizing the Street

Proposed:

- Narrow 10' travel lanes for traffic calming
- Two-way left turn lane for safer left turns
- Two-way cycle track with physical separation
- Areas for landscaped medians
- Areas for pedestrian refuge islands
- Areas for enhanced bus stops
- Reconstructed sidewalks vary in width from 6.5' to 8'

Proposed Design

E FLORIBRASKA AVE



Right-sizing the Street

Proposed:

- Narrow 10' travel lanes for traffic calming
- Two-way left turn lane for safer left turns
- Buffered and Protected Bike Lanes
- ~~Two-way cycle track with physical separation~~
- ~~Areas for landscaped medians~~
- ~~Areas for pedestrian refuge islands~~
- ~~Areas for enhanced bus stops~~
- ~~Reconstructed sidewalks vary in width from 6.5' to 8'~~

Concrete Bumper Guards



Floribraska Ave Complete Streets



Existing



Proposed

How to contact us?

- The City has developed an easy to remember project specific email address that residents can use to send comments or questions:
FloribraskaProject@tampagov.net
- The City asks that all comments be received no later than Friday, March 03, 2023 so that they may be considered in time for the Final Plans submittal.



<https://www.tampagov.net/tss-transportation/info/projects/FloribraskaAve>

QUESTIONS