



TAMPA MOVES

Twiggs Street

Workshop Summary and Report

October 2023

TAMPA
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Mobility • Opportunity • Vision • Equity • Safety

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PROJECT OVERVIEW AND PURPOSE

Through the citywide mobility plan Tampa M.O.V.E.S. and Vision Zero Action Plan, the City identified several corridors in need of safety and multimodal improvements to enhance the walking, rolling, biking, and driving experience along Tampa's roadways. Twiggs Street in downtown Tampa was selected for several reasons:

- ▶ Twiggs Street is on the High Injury Network, meaning it sees a disproportionately high number of fatal and life-altering crashes.
- ▶ It provides east to west connectivity through the entire downtown.
- ▶ The County Incentive Grant Program (CIGP) and the Safe Street for All grants provided a path forward for short-term implementation.

The Twiggs Street project, shown in Figure 1, encompasses the 1-mile stretch between Ashley Drive and Channelside Drive. Major destinations along this corridor include the Channel District, Tampa Union Station, Hillsborough County services, Franklin Street, and Curtis Hixon Park. Twiggs also serves as a historic street that once housed many of Tampa's original retail stores through the 1920's and 1930's such as Haverty's and Tarr's furniture stores. Today, the street continues to provide connections to Union Station and major highways like the Selmon Expressway and is one of only a handful of streets that connects the Hillsborough River with Port Tampa Bay. The vehicle volumes on Twiggs Street average 8,200 cars daily over the course of a year.

In recent years, Twiggs Street has captured the attention of the City due to the high number of crashes. Through the Vision Zero Action Plan, the segment between Nebraska Avenue and Channelside Drive was identified as part of the High Injury Network (HIN). The HIN denotes streets in the City that have a higher rate of crashes compared to other streets. Between 2014 and 2018, there were eight crashes with life altering injuries, two of which involved a pedestrian. Intersection safety is also a concern – seven of the eight most severe crashes happened at intersections.

The purpose of this study is to collect feedback and insight from critical stakeholders who have a vested interest in the corridor, as well as input from the general public who live and work within the study area. This study is meant to guide the funding dollars received from the County Incentive Grant Program (CIGP) and the Safe Street for All grant towards implementing a safer Twiggs Street.



Tarr's Furniture at Twiggs Street and Tampa Street, 1921 | Burgert Brother's Photographic Collection



Figure 1 Twigg Street Crashes

WORKSHOP SUMMARY

A design charrette was held on March 21st and March 22nd, 2023 at the Tampa Union Station. A design charrette is a collaborative public workshop that allows attendees to be the planners and engineers. The following sections describe the input gathered from key stakeholders and the general public.

Stakeholder Meetings

The City met one on one with the following groups before, during, and/or after the charrette:

- ▶ Hillsborough Transportation Planning Organization (TPO)
- ▶ Tampa Hillsborough Expressway Authority (THEA)
- ▶ Hillsborough County City Planning Department
- ▶ Hillsborough County Facilities Management & Real Estate Services
- ▶ Channelside Academy of Math and Science (Channelside Academy)
- ▶ Tampa Downtown Community Redevelopment Area (CRA)
- ▶ Tampa Downtown Partnership
- ▶ Tampa Electric Company (TECO)

Efforts were also made to coordinate with the United States General Service Administration (GSA) and Gas Worx, both stakeholders for the Twiggs Street Project. As they were not available during the stakeholder meeting period, their input is not included at this time.

During the outreach, multiple challenges and opportunities were identified by community members and stakeholders. Major themes and comments received through the stakeholder meetings included:

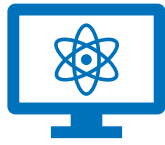
- ▶ Slowing down vehicle traffic
- ▶ Preserving parking for the businesses affronting Twiggs Street
- ▶ Adding additional crossing opportunities around the Courthouse and throughout the corridor
- ▶ Improving pedestrians' comfort, especially between Nebraska Avenue and Channelside Drive
- ▶ Mitigating conflict points between vehicles entering and exiting Twiggs Garage in front of the Courthouse
- ▶ Increasing shade all along Twiggs Street
- ▶ Limiting impacts to the Selmon Expressway Reversible Express Lanes

Meeting notes for each stakeholder meeting can be found in the Appendix A.

Attendance Summary



30+ in-person attendees over two days



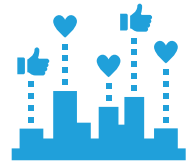
15+ attendees using online tools at www.tpamoves.org



4 stakeholder interviews



50+ contributions to the placemaking exercise



8 contributions to typical section exercise

Visioning Exercise: Placemaking

Attendees were encouraged to place sticky notes on a map with things they wanted to see on Twigg's Street. While many comments centered around programming and event-specific efforts, other comments were focused on the placemaking of the street. In conversation, participants wanted to see a connected network of multimodal facilities along the corridor that smoothly transitioned from one section to another. Other participants were heavily focused on protecting bicyclists through formal bicycle facilities and improving crossings, especially where trail crossings were found. Many wanted to maintain the current amount of parking along the corridor, particularly near restaurants and government uses. In terms of land use, there were recommendations to activate building frontages with public art, in addition to active streetscapes as an extension of public space.



Specific comments include:

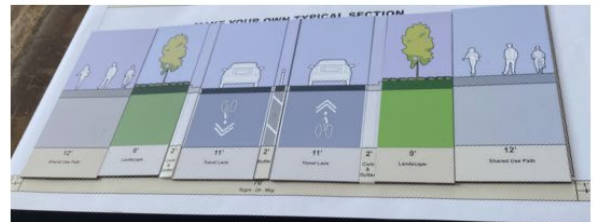
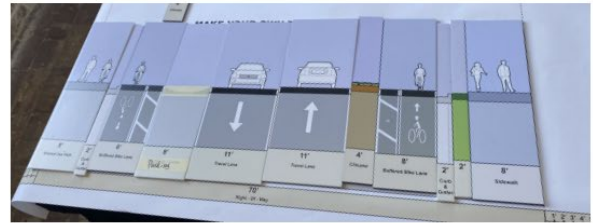
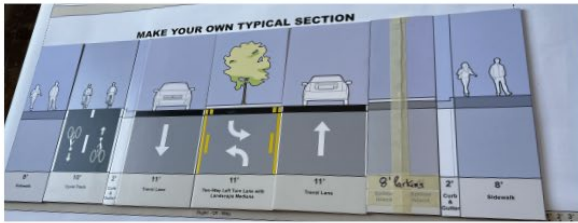
- ▶ Connecting Curtis Hixon Park to Twiggs Street, planning for connection
- ▶ Dog + human fountains for Instagram
- ▶ Closing the road for cars to open it for walk-bike-dogs, one day every month
- ▶ Public Art, and public accessible foundations!
- ▶ Programmed events
- ▶ Singles parklet
- ▶ Socialize the space from online communities
- ▶ “Instagrammable” places
- ▶ Street level activation at the Frontier building
- ▶ Street furniture, bulbouts, protected bike lanes, pedestrian signs
- ▶ Decrease lane widths
- ▶ Lighting for landscape
- ▶ Wider sidewalks
- ▶ Retractable bollards
- ▶ Bike lanes (protected)
- ▶ Underline park under expressway
- ▶ Eyes on the street
- ▶ Beautification of the underpass
- ▶ Partnership with Publix
- ▶ More green space
- ▶ Rubber tire streetcar
- ▶ Policing aggressive drivers
- ▶ Sidewalk on northside of Twiggs near station
- ▶ Less parking #bankrupt717
- ▶ Program public markets
- ▶ More parks for adults, not just playgrounds
- ▶ Better lighting
- ▶ Allow retail or markets under the Selmon Expressway



Twiggs Street Visioning exercise with community members

Visioning Exercise: Typical Sections

Attendees were also able to reimagine Twiggs Street by creating new typical sections. There were a total of 8 typical sections shared with the project team. Almost all sections reduced the number of lanes in the four-lane section and recommended intersection treatments at key crossing locations. All typical sections recommended some sort of bicycle facility with ranges of separation. These typical sections also expanded the current sidewalk and added elements such as street trees.



Key Recommendations

Based on the data reviewed and input gathered during the charrette, the project team developed recommendations for four-character areas. These include the following, shown in Figure 2:

- (1) Downtown Foot Traffic – Ashley Drive to Marion Street
- (2) Transition Zone & County Services – Florida Avenue to Nebraska Avenue
- (3) Union Station - Nebraska Avenue to Meridian Avenue
- (4) Channel District – Meridian Avenue to Channelside Drive

The section of Twiggs Street from Nebraska Avenue to Meridian Avenue has a complex vehicle pattern and is under further study. A typical section is not recommended at this time.

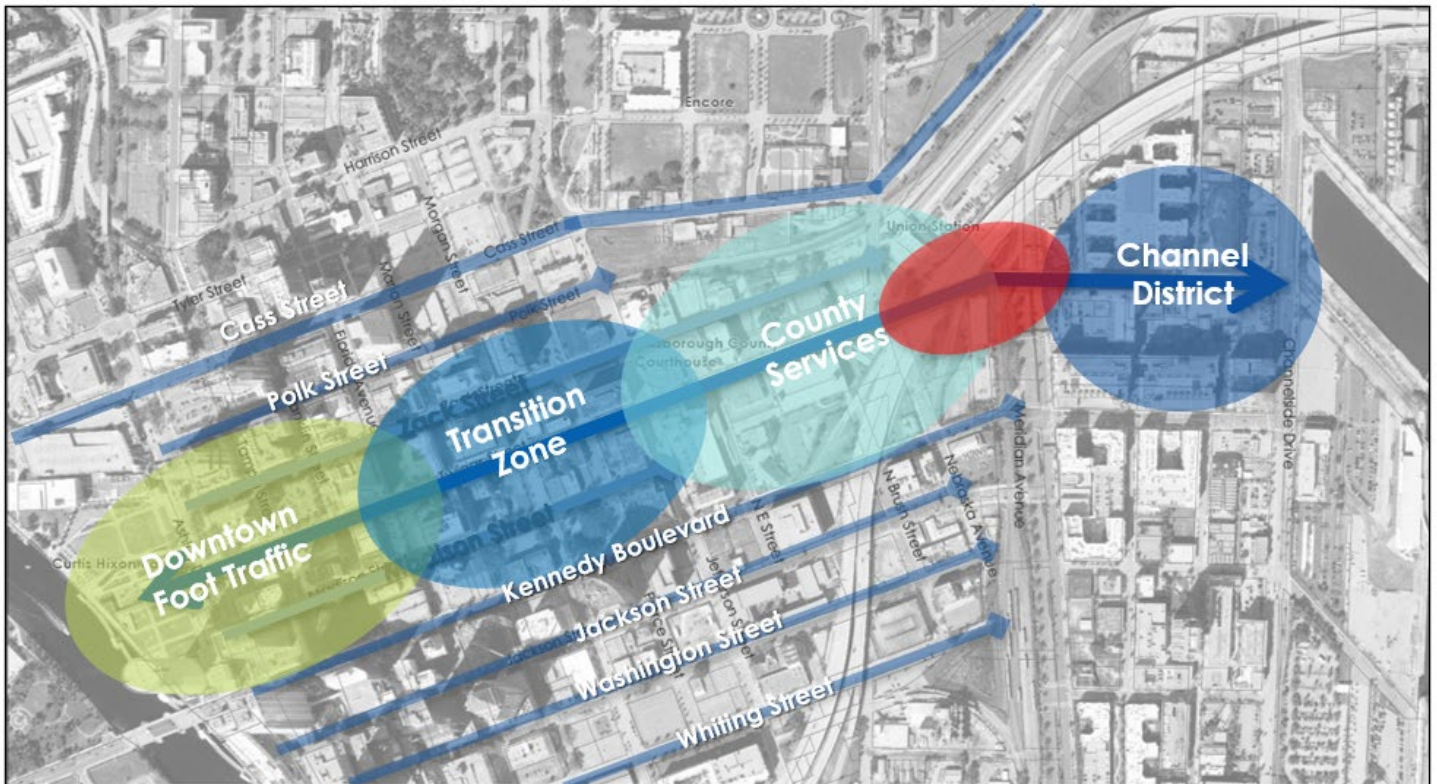


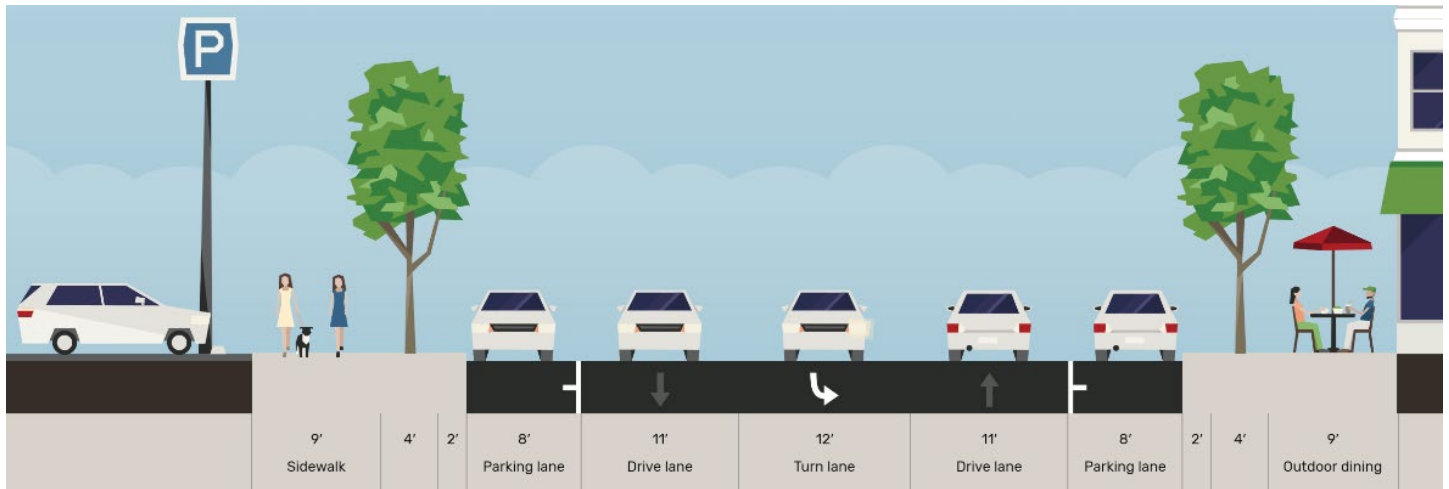
Figure 2 Character Areas of Twiggs Street

The following sections detail recommendations for the context areas noted above.

Downtown Foot Traffic Area

Typical Section: Ashley Drive to Florida Avenue

Below shows the current typical section for this area:

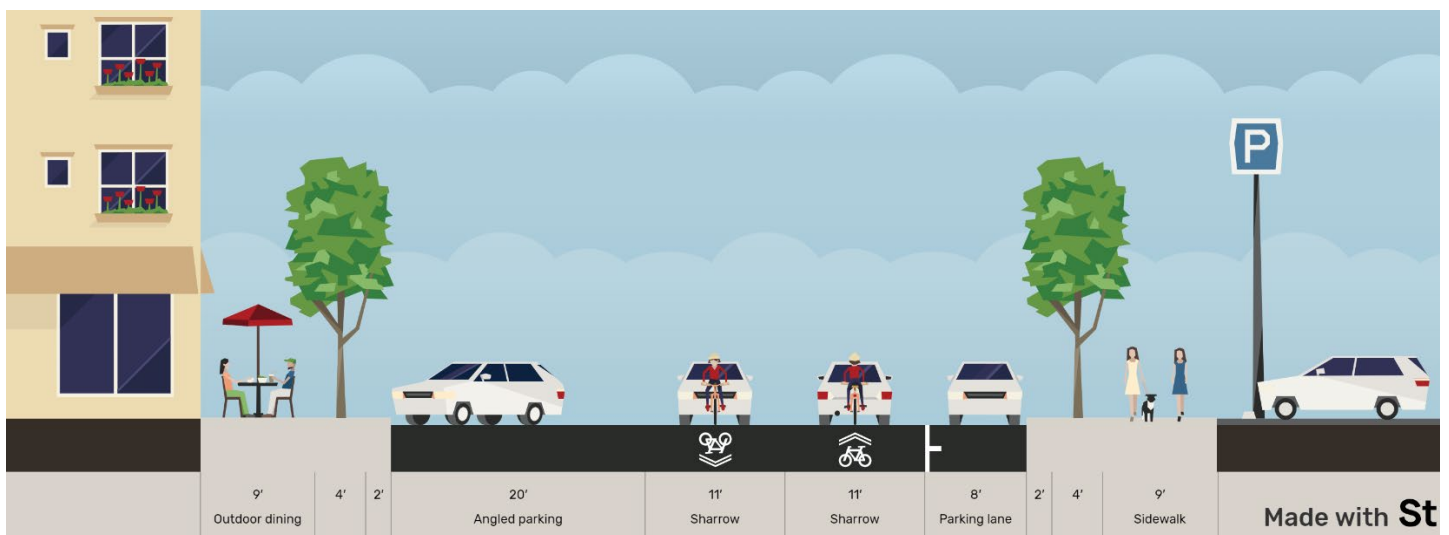


Workshop comments in this area included the following:

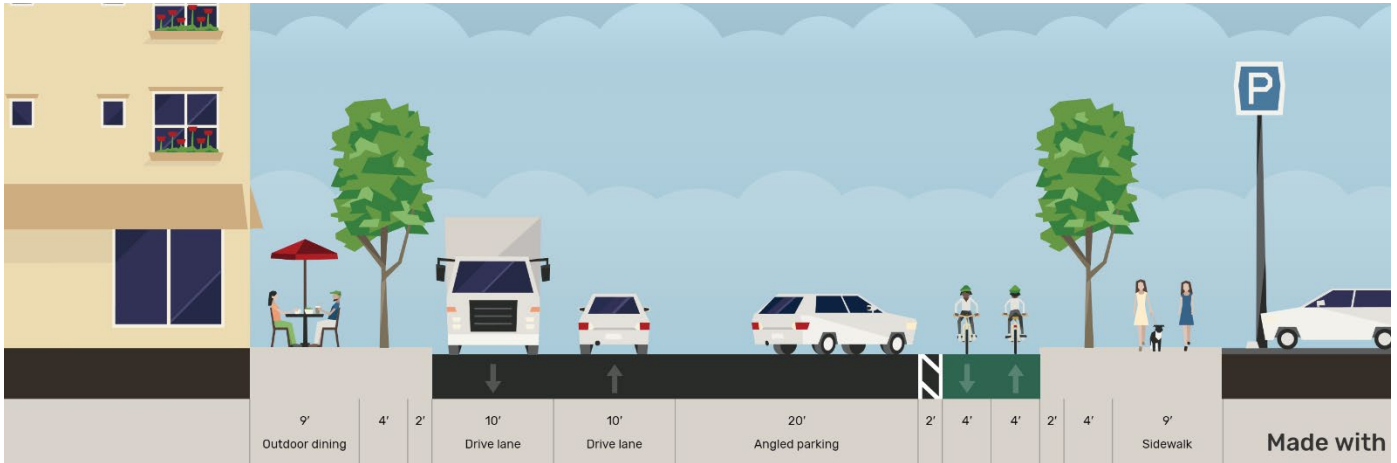
- ▶ Preserve the bricked streets and street mural at the intersection of Twiggs Street and Franklin Street
- ▶ Preserve parking for the businesses on Twiggs Street
- ▶ Provide a means of bicyclist travel on-street, whether by sharing a lane or a bicycle lane

Based on the feedback provided by the public and stakeholders, the regulatory requirements from City code, and the existing ROW, the following typical section options are recommended for this area. The desired outcome is to reduce vehicles speeds to a point that where bicyclists and motorists can comfortably share a lane while maximizing parking for businesses on Twiggs Street. The typical section shown is looking westbound from Florida Avenue.

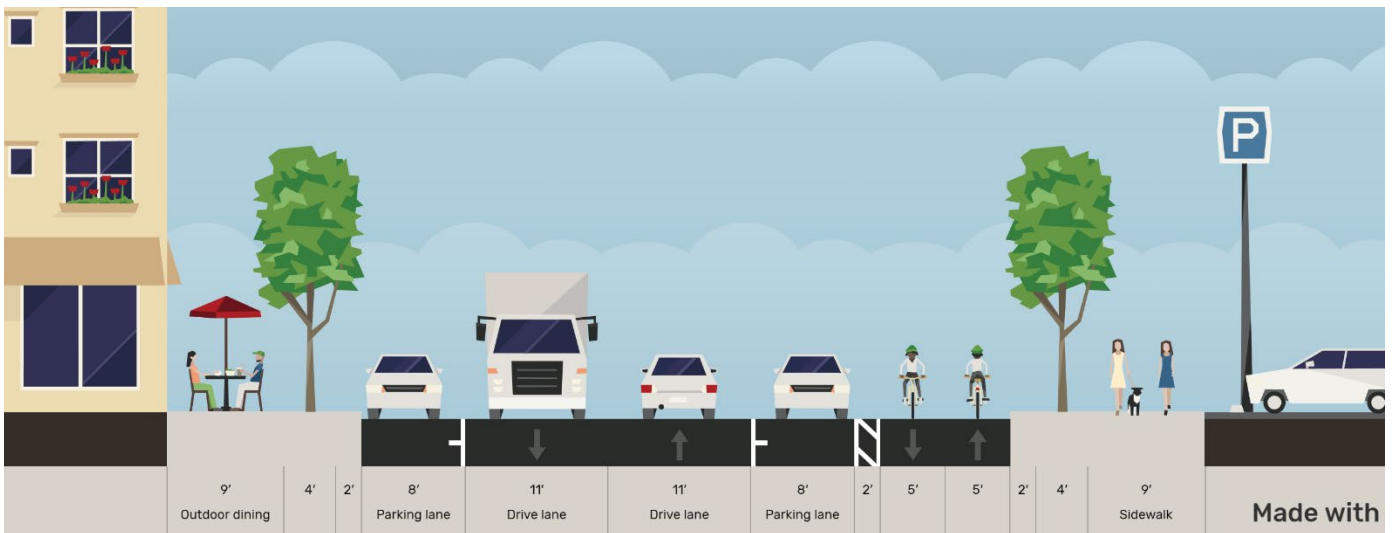
Option 1: Removal of turn lane, addition of angled parking on north side, shared lane with bicycle traffic



Option 2: Removal of turn lane, add angled parking on the southside, add cycle track on the south side



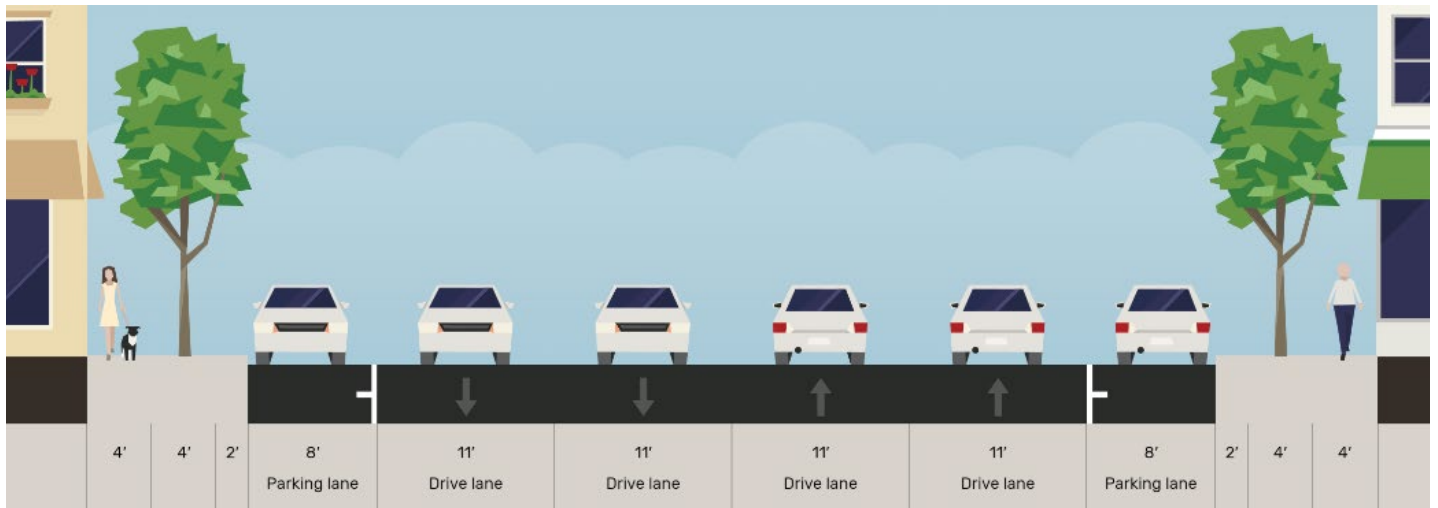
Option 3: Maintain parallel parking, add cycle track to south side, remove turn lane



Transition Zone & Government Services

Typical Section: Florida Avenue to Nebraska Avenue

Below shows the current typical section for this area:



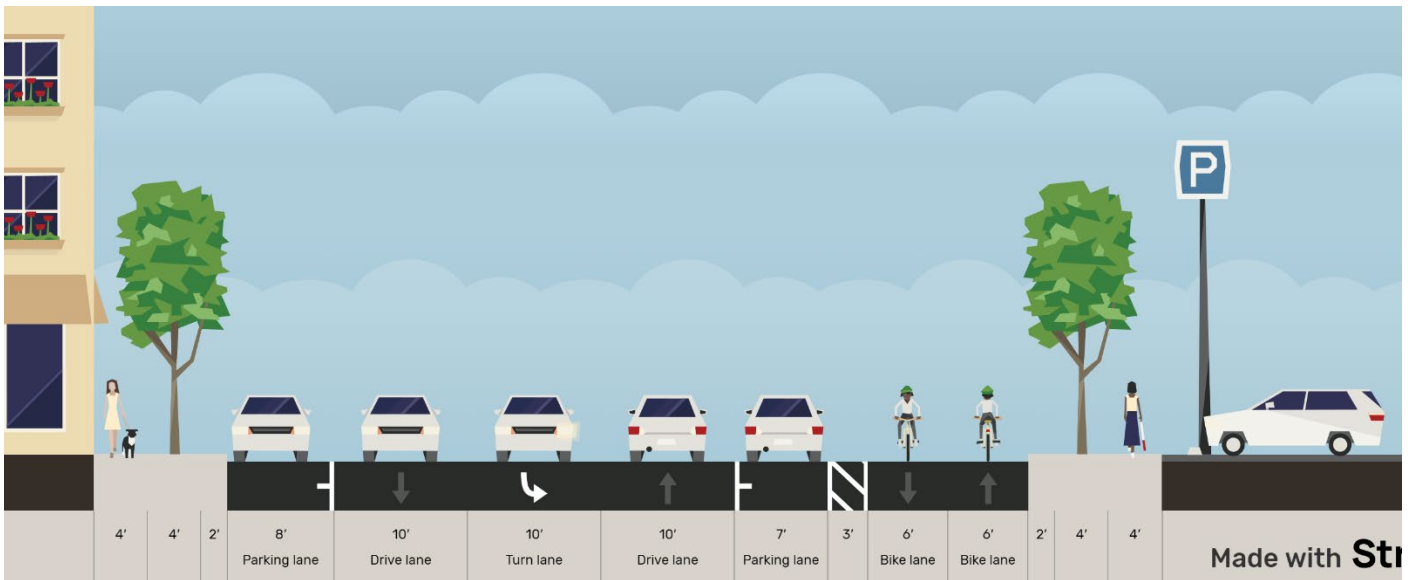
Comments received for this area include the following:

- ▶ Give character to the area through public arts programming and activating areas in the ROW with furnishings
- ▶ Use this area to facilitate travel to transit stops
- ▶ Provide parking to serve government uses
- ▶ Increase shade and pedestrian comfort
- ▶ Increase crossing opportunities

This area of Twiggs Street maintains the B street typology guidelines in City code. Based on the feedback provided by the public and stakeholders, the regulatory requirements from City code, and the existing ROW, the following typical section options are recommended for this area. The typical section shown is looking westbound from Nebraska Avenue.

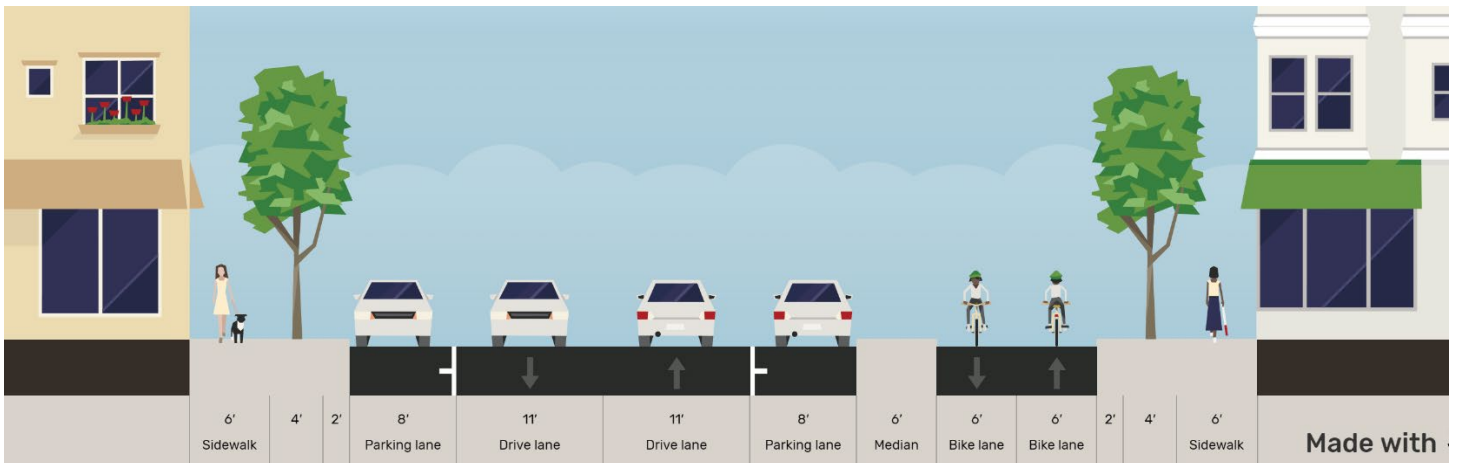
The first option is a more short-term option that repurposes a lane in each direction to create a two-way separated bicycle facility and a center left turn lane with spot medians. This option preserves the existing curb.

Option 1: Repurpose lanes to create a two-way separated bicycle facility and center left turn lane.



The second option is a longer-term option that moves the curb to widen the sidewalk, removing the center left turn-lane. Both options preserve the existing on-street parking.

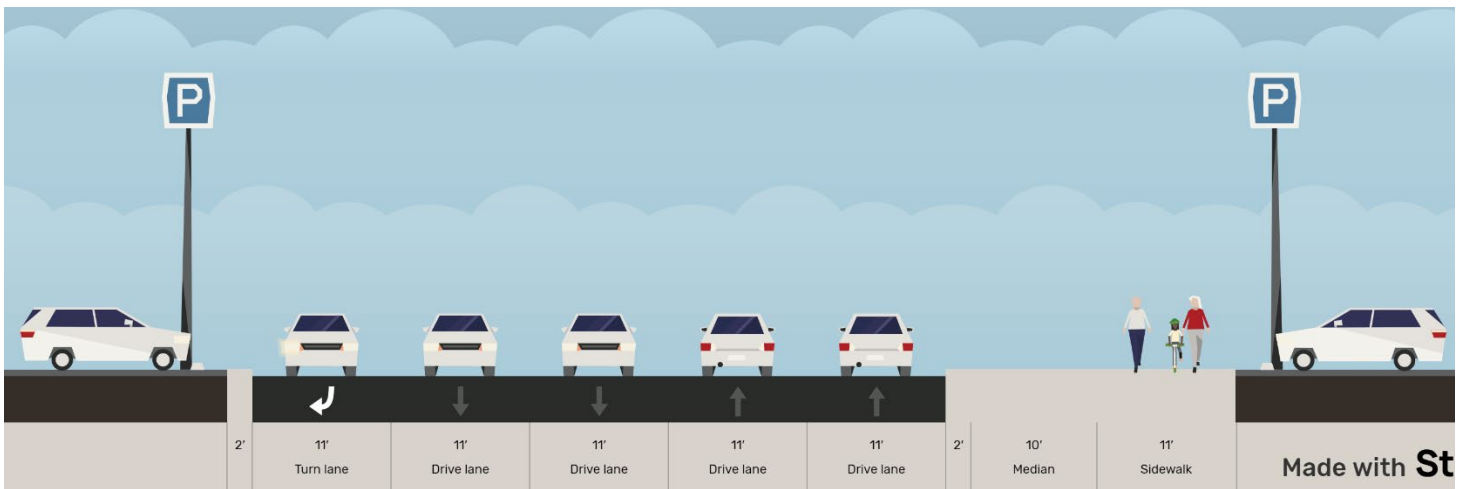
Option 2: Remove two through lanes, add parking protected bicycle lanes, expansion of sidewalk to 6 feet. Flex posts can also be a curbed barrier (shown).



Union Station

Typical Section: Nebraska Avenue to Meridian Avenue

Below shows the current typical section for this area:



Comments for this area included the following:

- ▶ Address safety issues related to trail crossings as well as critical crossings at Nebraska Avenue and Meridian Avenue
- ▶ Improve school circulation and safety by converting the school driveways to right-in/right-out only
- ▶ Mitigate impact to the Selmon Expressway Reversible Express Lanes

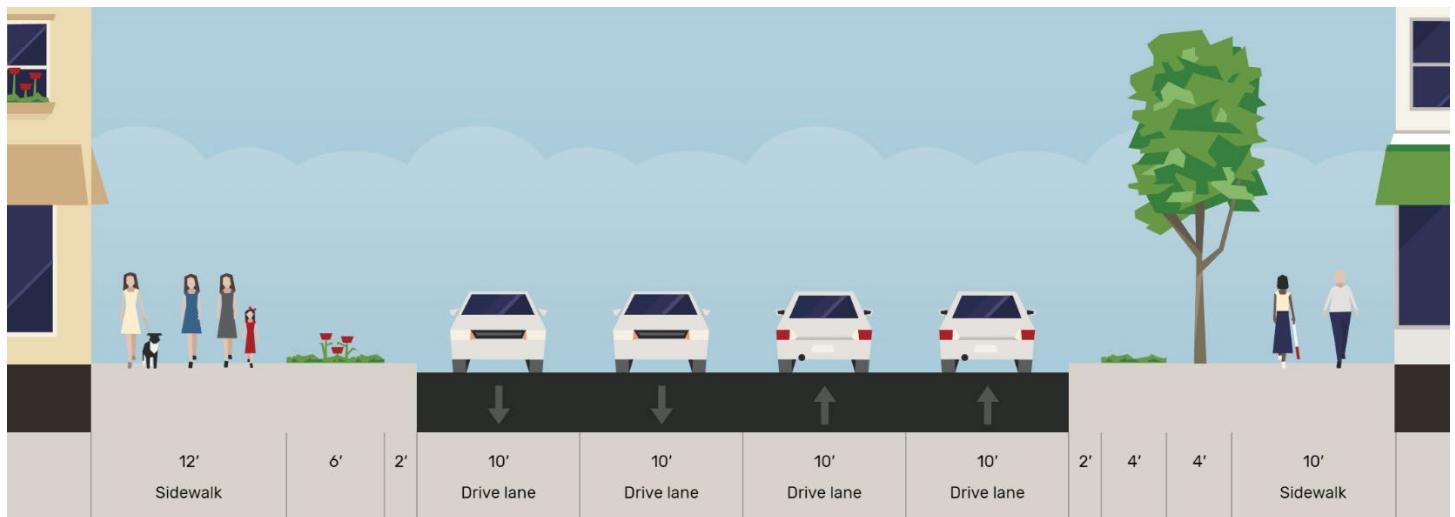
This area of Twiggs Street maintains the B street typology guidelines in City code. Because of the complex vehicle pattern between the Selmon Expressway Reversible Express Lanes and Nebraska, this section is under further and a typical section is not recommended at this time.

Options for Nebraska Avenue intersection configurations can be found in the Traffic Operations section.

Channel District

Typical Section: Meridian Avenue to Channelside Drive

Below shows the current typical section for this area:

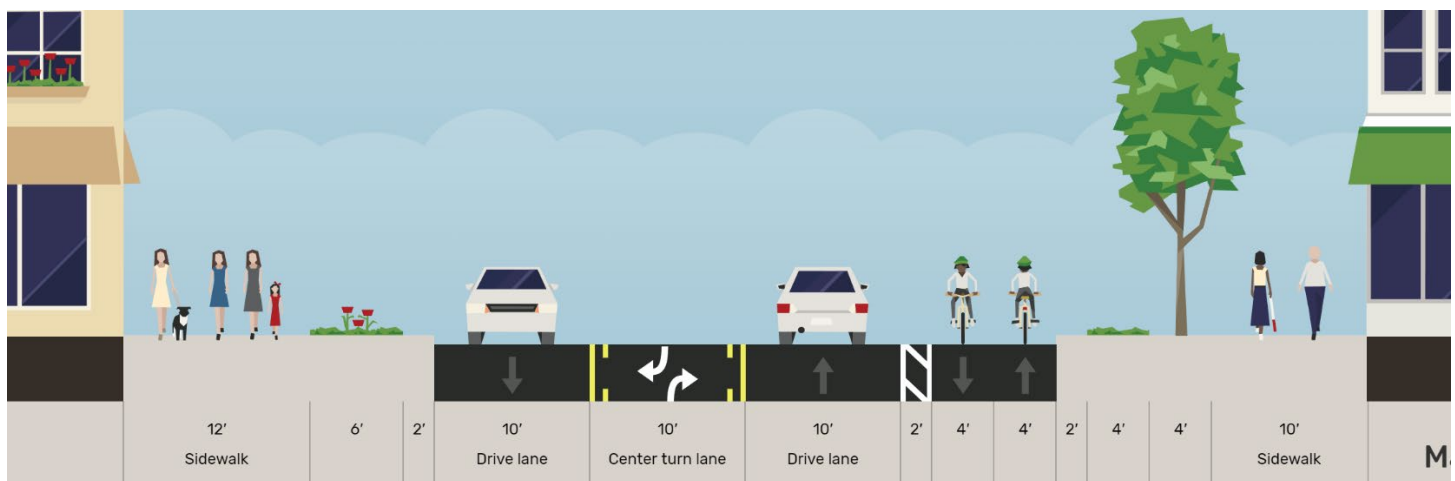


Those who contributed typical sections for this area included the following:

- ▶ Provide safe crossing locations for pedestrians, especially at trail locations
- ▶ Increase connectivity to parks
- ▶ Traffic calming
- ▶ Protected bicycle facilities
- ▶ Maintain or increase parking

This area of Twiggs Street is located within the Channel District CRA. The CRA is working on a project to enhance the existing crosswalk with a refuge island. The typical section below is meant to be work with and enhance the already established plans.

Recommendation: Two lane section, adds two-way cycle track on south side



Intersection Treatments

Within each context area, it is recommended that intersection treatments work to protect multimodal users while providing for the appropriate flow of traffic. Bulbouts and curb extensions can work to shorten the crossing distance for pedestrians and slow turning vehicles around corners. This can be coupled with raising the intersection to improve the line of sight during the intersection approach and slow vehicles down as they travel through the intersection. It is also recommended that the signal at Franklin Street be evaluated for replacement with a four-way stop to prioritize pedestrian movements. Below are some of these treatments applied in other areas.



Figure 2 Scooter Corral in a Curb Extension, Orlando, FL



Figure 3 Example of a Raised Intersection | National Association of City Transportation Officials (NACTO)

TRAFFIC OPERATIONS

The intersection at Twiggs Street and Nebraska Avenue underwent additional traffic analysis to understand how a potential lane repurposing would impact vehicle travel. Two alternative scenarios were evaluated for feasibility, as seen in Table 1 and Table 2:

1. Remove an eastbound thru lane, creating one shared left/thru/right lane in the eastbound direction.
2. Convert the two existing eastbound thru lanes to one exclusive left-turn lane and one shared thru/right lane. This option would require eliminating approximately six on-street parking spaces to preserve the bicycle facility shared in the recommended typical section.

Based on the level of traffic coming off the Selmon Expressway Reversible Express Lanes in the morning, a westbound lane reduction was not considered, to avoid additional queue spill back on the Expressway.

The City of Tampa aims to keep volume to capacity (v/c) ratios below 1.25. A v/c ratio estimates the level of congestion on a roadway with a v/c ratio of 1.00 being the theoretical capacity. A v/c ratio greater than one may result in congestion for longer periods of time in the peak hour, which is expected in downtown environments.

The traffic analysis reviewed 2045 volumes provided by THEA. Based on the 2045 volumes, the intersection is expected to operate within a 1.25 v/c ratio in both morning and evening peaks, with v/c ratios of 1.09 and 1.22, respectively, for the worst movement.

Reducing the eastbound travel lanes from two lanes to one lane results in a v/c ratio of 1.25 for the westbound right-turn lane in the morning; however, congestion is expected to get significantly worse in the evening with a v/c ratio of 1.98 for the eastbound left. Adding an exclusive left turn lane would allow the signal to operate with an acceptable level of congestion in the morning, with almost no difference in delay compared to the existing configuration. In the evening, the v/c ratio for the eastbound left-turn lane is reduced to 1.45 under this scenario.

Table 1 Traffic Operation Analysis of Twiggs Street & Nebraska Avenue AM Peaks

Design Year (2045) AM					
Alternative	Approach	Control Delay	Level of Service	Max V/C	Max V/C Movement
No Build	EB	22.2s	C	0.30	EBL
	WB	17.7s	B	1.09	WBR
	NB	47.8s	D	0.69	NBR
	SB	29.9s	C	0.51	SBL
	Overall	24.1s	C	1.09	WBR
Eastbound One Shared Left/Thru/Right Lane	EB	29.7s	C	0.47	EBL
	WB	55.1s	E	1.25	WBR
	NB	37.5s	D	0.58	NBR
	SB	23.8s	C	0.46	SBL
	Overall	45.9s	D	1.25	WBR
Eastbound One Left Turn Lane & One Shared Thru/Right Turn Lane	EB	23.2s	C	0.29	EBR
	WB	13.9s	B	1.07	WBR
	NB	49.4s	D	0.70	NBR
	SB	31s	C	0.53	SBL
	Overall	22.2s	C	1.07	WBR

Table 2 Traffic Operation Analysis of Twiggs Street & Nebraska Avenue PM Peaks

Design Year (2045) PM					
Alternative	Approach	Control Delay	Level of Service	Max V/C	Max V/C Movement
No Build	EB	116.2s	F	1.22	EBL
	WB	47.4s	D	0.34	WBL
	NB	68.3s	E	0.81	NBR
	SB	43.7s	D	0.96	SBL
	Overall	77.8s	E	1.22	EBL
Eastbound One Shared Left/Thru/Right Lane	EB	494.6s	F	1.98	EBL
	WB	42.6s	D	0.25	WBL
	NB	68.3s	E	0.81	NBR
	SB	43.7s	D	0.96	SBL
	Overall	237.2s	F	1.98	EBL
Eastbound One Left Turn Lane & One Shared Thru/Right Turn Lane	EB	228.9s	F	1.45	EBR
	WB	48.7s	D	0.42	WBL
	NB	68.3s	E	0.81	NBR
	SB	43.7s	D	0.96	SBL
	Overall	125.6s	F	1.45	EBR

The City also conducted a traffic operation analysis considering removal of the southbound right-turn slip lane at Nebraska Avenue. Tables 3 and 4 show their results for existing conditions, removing the southbound slip right turn lane, and with southbound dual left turn lanes.

Additional traffic operation analyses for Nebraska Avenue can be found in the Appendix B.

Table 3 Southbound Approach Configuration Conditions Comparison Results at Twiggs Street and Nebraska Avenue - AM Peak Hour

Configuration Condition		Synchro Analysis SB Queue Length (Feet)			Synchro Simulation SB Queue Length (Feet)	
		50% (percentile)	95% (percentile)	LOS	50% (percentile)	95% (percentile)
Existing (2022)	Left turn lane	56	139	D	74	110
	Thru lane 1	59	115	D	22	42
	Thru lane 2				37	87
	Slip Right turn lane	12	79	B	34	50
Without Southbound Slip Right Turn Lane	Left turn lane	56	139	D	55	69
	Thru lane 1				51	137
	Thru + Right turn lane	51	135	C	75	145
With Southbound Dual Left Lanes	Left lane 1	28	65	F	43	70
	Left lane 2				13	45
	Thru lane	114	221	D	106	187
	Right turn lane	12	79	B	37	55

Table 4 Southbound Approach Configuration Conditions Comparison Results at Twiggs Street and Nebraska Avenue - PM Peak Hour

Configuration Condition		Synchro Analysis SB Queue Length (Feet)			Synchro Simulation SB Queue Length (Feet)	
		50% (percentile)	95% (percentile)	LOS	50% (percentile)	95% (percentile)
Existing (2022)	Left turn lane	274	584	F	236	262
	Thru lane 1	52	74	B	217	465
	Thru lane 2				31	62
	Slip Right turn lane	3	13	A	16	51
Without Southbound Slip Right Turn Lane	Left turn lane	274	584	F	245	245
	Thru lane 1				372	409
	Thru + Right turn lane	57	83	B	72	117
With Southbound Dual Left Lanes	Left lane 1	203	330	F	206	208
	Left lane 2				243	248
	Thru lane	114	172	C	337	590
	Right turn lane	3	15	A	6	25

CONCLUSION

Utilizing the existing challenges and opportunities along Twiggs Street as well as the information gathered from the design charrette, stakeholder meetings, and City staff meetings, recommendations and improvements were identified for implementation.

Summary of Outreach

Challenges and opportunities were identified during the design charrette and stakeholder meetings. Overall, community members and stakeholders expressed interest in:

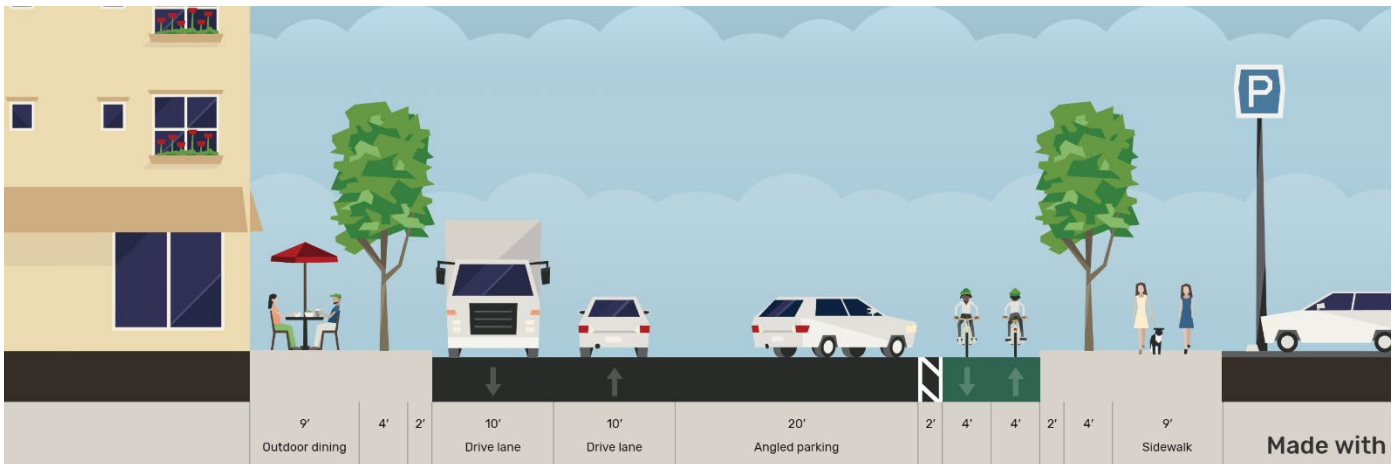
- ▶ Slowing down vehicle traffic
- ▶ Preserving parking for the businesses affronting Twiggs Street
- ▶ Adding additional crossing opportunities around the Courthouse and throughout the corridor
- ▶ Improving pedestrians' comfort, especially between Nebraska Avenue and Channelside Drive
- ▶ Mitigating conflict points between vehicles entering and exiting Twiggs Garage in front of the Courthouse
- ▶ Increasing shade all along Twiggs Street
- ▶ Limiting impacts to the Selmon Expressway Reversible Express Lanes
- ▶ Adding and connecting parks and greenspaces
- ▶ Improved lighting throughout the corridor

Summary of Recommendations for City Review

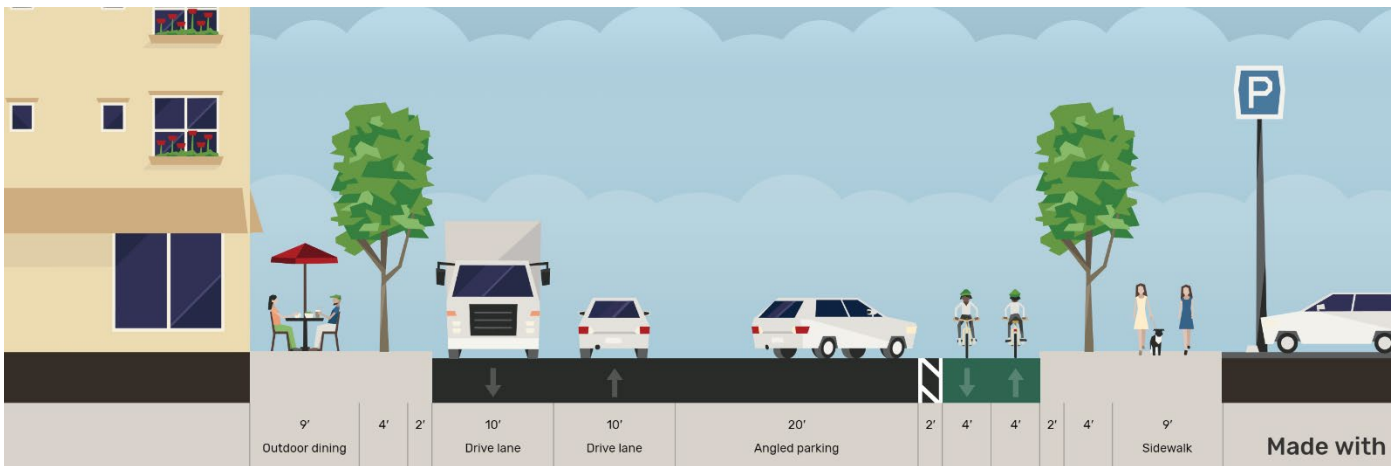
Typical sections were used to reimagine Twiggs Street and almost all sections reduced the number of lanes in the four-lane section and recommended intersection treatments at key crossing locations. All typical sections recommended some sort of bicycle facility with ranges of separation. The primary goal for the project is to create a continuous bicycle facility through all four character areas.

(1) Downtown Foot Traffic – Ashley Drive to Florida Avenue

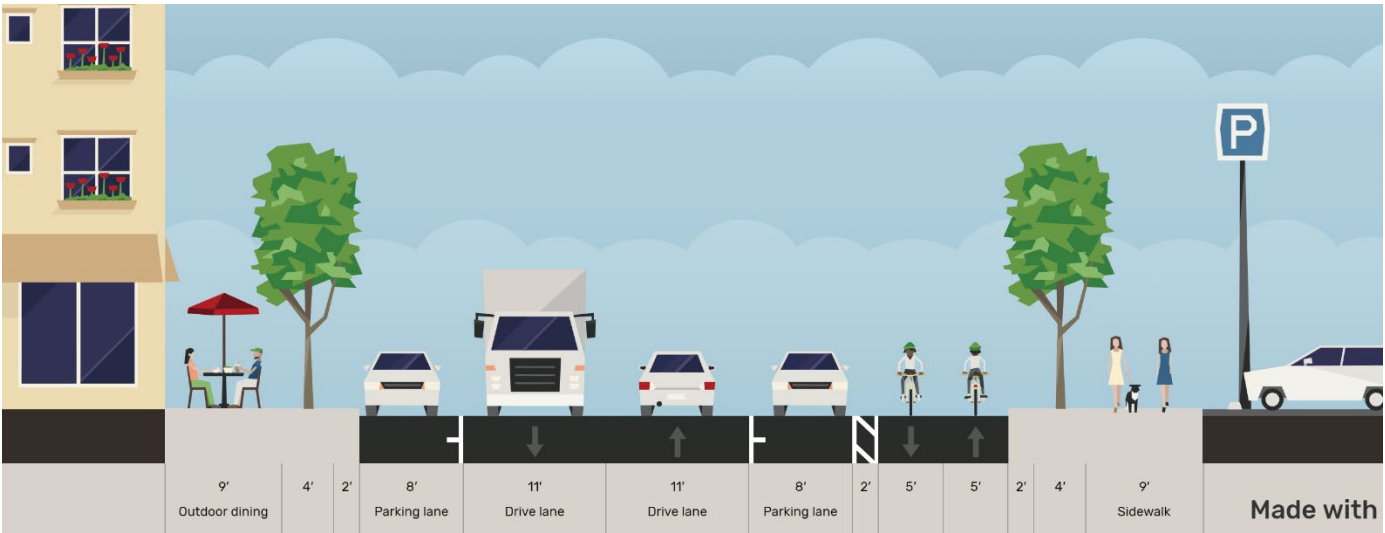
Option 1: Removal of turn lane, add angled parking on the southside, add cycle track on the south side



Option 2: Removal of turn lane, add angled parking on the southside, add cycle track on the south side

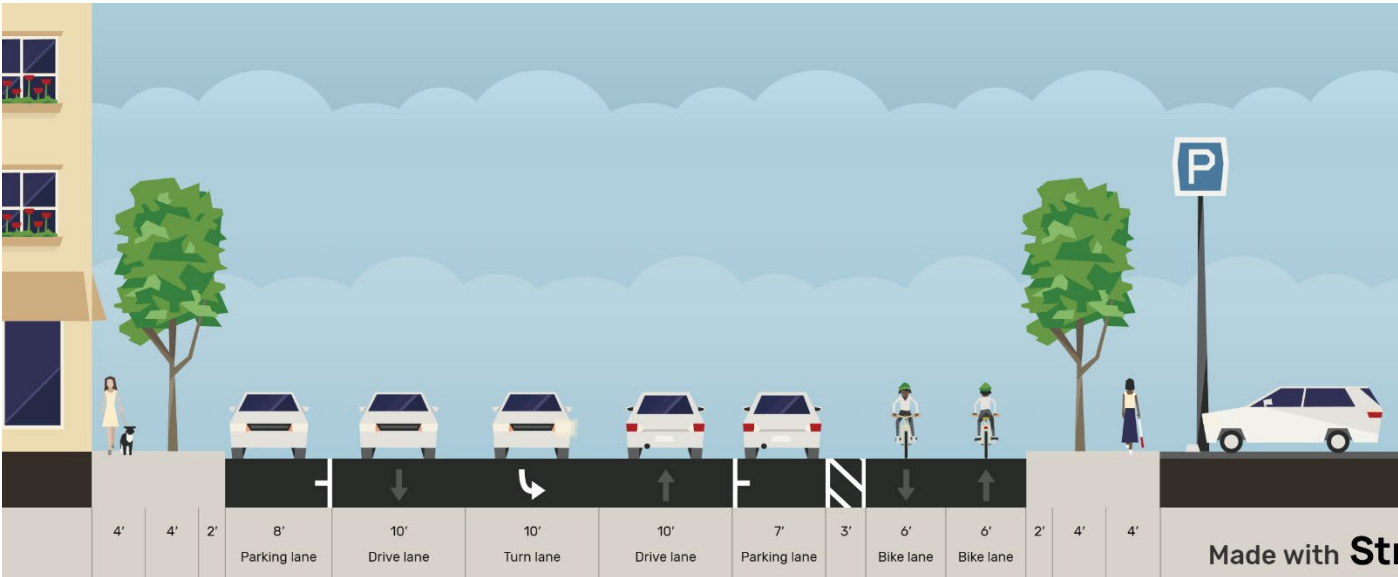


Option 3: Maintain parallel parking, add cycle track to south side, remove turn lane

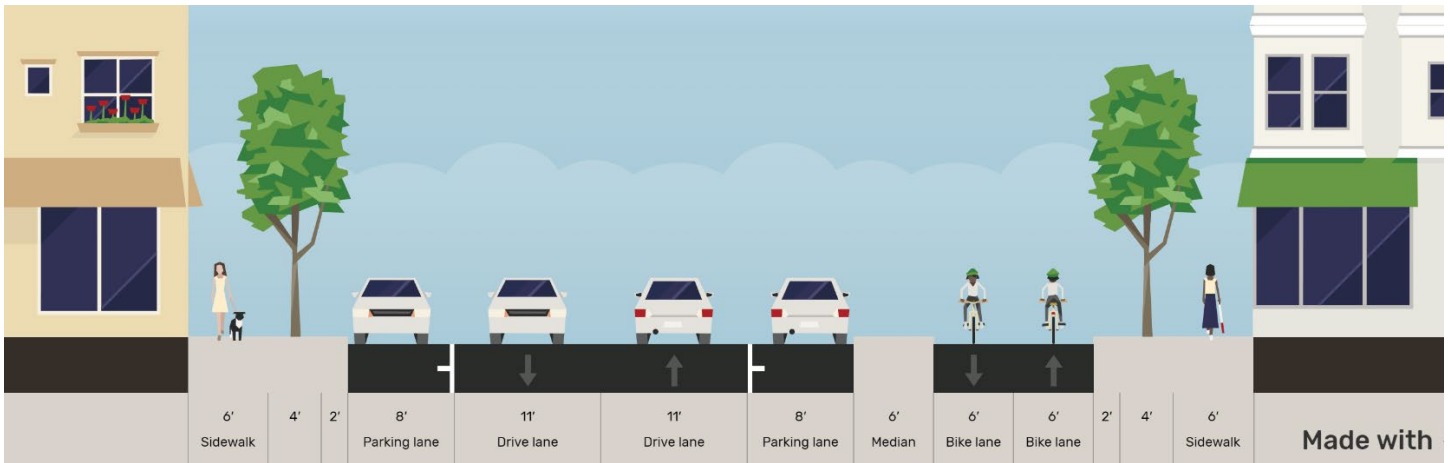


(2) Transition Zone & County Services – Florida Avenue to Nebraska Avenue

Option 1: Repurpose lanes to create a two-way separated bicycle facility and center left turn lane.



Option 2: Remove two through lanes, add parking protected bicycle lanes, expansion of sidewalk to 8 feet. Flex posts can also be a curbed barrier.

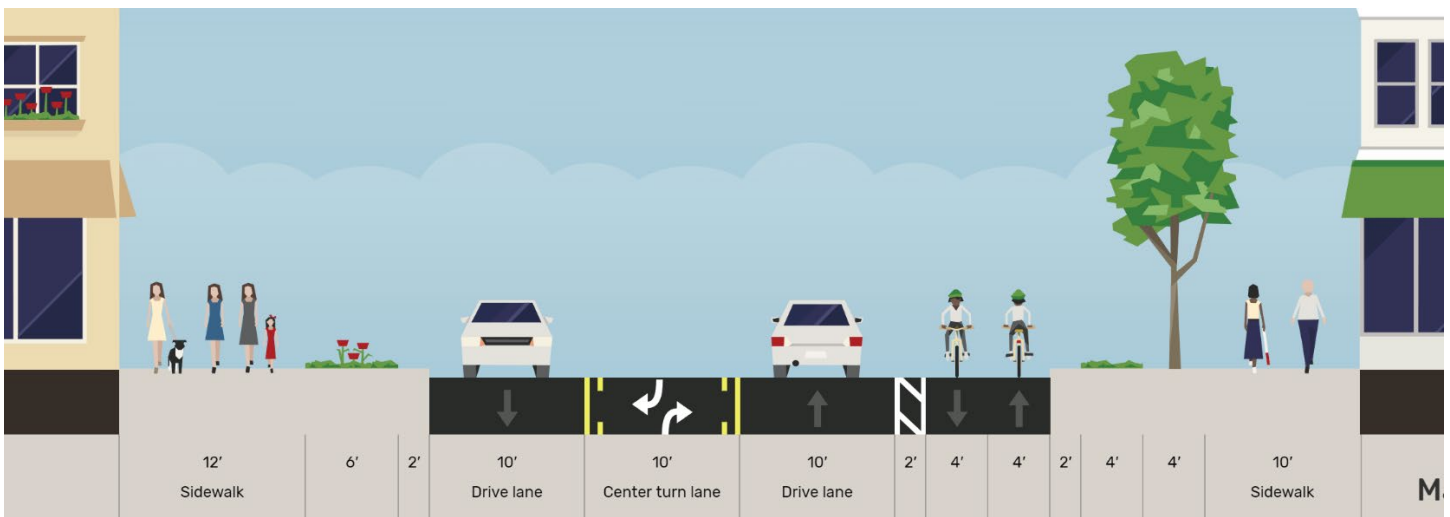


(3) Union Station - Nebraska Avenue to Meridian Avenue

This area of Twiggs Street maintains the B street typology guidelines in City code. Because of the complex vehicle pattern between the Selmon Expressway Reversible Express Lanes and Nebraska, this section is under further and a typical section is not recommended at this time.

(4) Channel District – Meridian Avenue to Channelside Drive

Recommendation: Two lane section, addition of parking on southside, adds two-way cycle track on south side



Next Steps

These recommendations will be reviewed by the City of Tampa and be considered for further design for the Twiggs Street Project. The County Incentive Grant Program can address improvements for the section from Ashley Drive to Nebraska Avenue. Safe Streets for All (SS4A) grant funding may be used for improvements to the section from Nebraska Avenue to Channelside Drive.

APPENDIX

Appendix A: Stakeholder Meeting Notes

Downtown Community Redevelopment Agency (CRA)

Date: March 21st, 2023

Attendees:

- ▶ Jeff Burton, CRA
- ▶ Aiah Yassin, City of Tampa
- ▶ JP Weesner, Kittelson & Associates, Inc.
- ▶ Jennifer Musselman, Kittelson & Associates, Inc.
- ▶ Riva Heinrich, Kittelson & Associates, Inc.
- ▶ Jady Chen, Kittelson & Associates, Inc.

Meeting Summary

Aiah Yassin started the meeting by introducing the project team and gave an overview of the Twiggs Street Project. The CRA was asked to provide concerns or known issues along Twiggs Street as well as what they had envisioned for the corridor.

Some concerns included:

- ▶ Queueing during pick-up/drop-off at Channelside Academy
- ▶ Vehicular traffic moving towards Kennedy Avenue
- ▶ Florida Avenue and Tampa Avenue are hard to cross
- ▶ Property owners impacting land development, such as 717
- ▶ Not much stormwater treatment, water goes into Hillsborough River
 - Potential to add solar powered treatment

Partnership:

- ▶ CRA is interested in partnering with the City of Tampa
- ▶ CIP versus additional funding resources

General Notes:

- ▶ Twiggs Street can act as an east/west multi-modal crosswalk to connect Riverwalk to Channel District and potential Riverwalk extension
- ▶ Plans for Hillsborough Area Regional Transit (HART) to surrender Marion back to the city as a local street
- ▶ Interest in mixed market housing
- ▶ Want to see consistency with the rest of Downtown
- ▶ Twiggs Street has a neutral effect on businesses Downtown
- ▶ Other parallel streets to Twiggs Street that vehicular traffic can be re-directed to

Next Steps:

- ▶ Project Team to continue updating CRA

Tampa Downtown Partnership

Date: March 21st, 2023

Attendees:

- ▶ Jason Collins, ADEAS-Q, Transportation Engineering & Planning
- ▶ Jeff Houck, 1905 Restaurants
- ▶ Karen Kress, Director of Transportation and Planning, Tampa Downtown Partnership
- ▶ Aiah Yassin, City of Tampa
- ▶ JP Weesner, Kittelson & Associates, Inc.
- ▶ Jennifer Musselman, Kittelson & Associates, Inc.
- ▶ Riva Heinrich, Kittelson & Associates, Inc.
- ▶ Jady Chen, Kittelson & Associates, Inc.

Meeting Summary

Jennifer Musselman started the meeting by introducing the project team and gave an overview of the Twiggs Street Project. The Downtown Partnership and other stakeholders were asked to provide concerns or known issues along Twiggs Street as well as what they had envisioned for the corridor.

Interest in Twiggs Street:

- ▶ Crossing between Ybor and Tampa Heights
- ▶ Envision as a crossway for bicyclists and pedestrians or streetcar line
- ▶ Important connection to other neighborhoods

Some concerns included:

- ▶ Conflicts around Courthouse
- ▶ Increasing densities and development in Channel District
- ▶ Vehicular traffic heaviest on Mondays to Wednesdays because of Court trial schedules
- ▶ Lack of tolls Downtown may encourage more cut through
- ▶ Want for preserving Brightline connections and turning into multi-modal hub
- ▶ Disconnect between Channel District and Downtown Central Business District

General Notes:

- ▶ Hillsborough County Courthouse plans for increased parking
- ▶ Lane repurposing for west of Nebraska Avenue
- ▶ Extend brick further east
- ▶ Increase lighting

Next Steps:

- ▶ Project Team to follow-up with city about Twiggs Street garage and plans for redevelopment
- ▶ Project Team to continue updating Downtown Partnership

Hillsborough Transportation Planning Organization

Date: March 21st, 2023

Attendees:

- ▶ Gena Torres, Hillsborough Transportation Planning Organization (TPO)
- ▶ Aiah Yassin, City of Tampa
- ▶ JP Weesner, Kittelson & Associates, Inc.
- ▶ Jennifer Musselman, Kittelson & Associates, Inc.
- ▶ Riva Heinrich, Kittelson & Associates, Inc.
- ▶ Jady Chen, Kittelson & Associates, Inc.

Meeting Summary

Jennifer Musselman started the meeting by introducing the project team and gave an overview of the Twiggs Street Project. The TPO was asked to provide concerns or known issues along Twiggs Street as well as what they had envisioned for the corridor.

Some concerns included:

- ▶ Maintain existing brick section and extend if possible
- ▶ Twiggs Street Parking garage conflicts, want to prioritize the sidewalk pedestrian traffic
- ▶ Challenging to walk on sidewalk with indentation of tree grates
 - Consider ways to extend into bushes
 - Other ways to get an ADA compliant path
 - Want to maintain tree shade
- ▶ People need access to destinations
 - Cass Street may provide alternative
 - If speeds are slow, shared street possible for bicyclists
 - Need separation of bicyclists and pedestrians
- ▶ Prefer on-street parking and two lanes with pocket parks/parklets
- ▶ Murals at larger intersections and placemaking/gateway with theme to distinguish Twiggs Street
- ▶ Meridian Avenue is a high safety concern in the city for the TPO
- ▶ Need for more crossing opportunities
- ▶ Conflict with scooters and shared bicyclists on the sidewalk
- ▶ Need for sidewalk on north side of Twiggs Street between Nebraska Avenue and Meridian Avenue

Next Steps:

- ▶ Project Team to continue updating TPO

Tampa Hillsborough Expressway Authority (THEA)

Date: March 22nd, 2023

Attendees:

- ▶ Robert Frey, THEA
- ▶ Anna Quinones, THEA
- ▶ Christina Matthews, WSP
- ▶ Scarlett Sharp, WSP
- ▶ Alex Henry, City of Tampa
- ▶ Alana Braiser, City of Tampa
- ▶ Cal Hardie, City of Tampa
- ▶ Aiah Yassin, City of Tampa
- ▶ JP Weesner, Kittelson & Associates, Inc.
- ▶ Jennifer Musselman, Kittelson & Associates, Inc.
- ▶ Riva Heinrich, Kittelson & Associates, Inc.
- ▶ Jady Chen, Kittelson & Associates, Inc.

Meeting Summary

Jennifer Musselman started the meeting by introducing the project team and gave an overview of the Twiggs Street Project. THEA was asked to provide concerns or known issues along Twiggs Street as well as what they had envisioned for the corridor.

Some concerns included:

- ▶ Pedestrians travelling from Channel District to Downtown
- ▶ Need for more crossings
- ▶ Vehicular traffic growth occurring off peak

General Notes:

- ▶ Expressed interest in more opportunities to bring community together to partner with THEA
- ▶ Selmon Expressway changes
 - Rail spur around Union Station is planned to be replaced with a trail
 - WSP working on a trail plan
 - Adding safety treatments at crossings
- ▶ Need operational analysis for crossings at Nebraska Avenue, Meridian Avenue, or mid-block

Next Steps:

- ▶ Project Team to continue to update THEA
- ▶ WSP to provide trail plan
- ▶ THEA to explore alternative intersections at Meridian Avenue

Hillsborough County City Planning Department

Date: May 3rd, 2023

Attendees:

- ▶ Sarah Caper, Hillsborough County Executive Planner
- ▶ Rebecca Hessinger, Hillsborough County Executive Planner
- ▶ Aiah Yassin, City of Tampa
- ▶ Jennifer Musselman, Kittelson & Associates, Inc.
- ▶ Jady Chen, Kittelson & Associates, Inc.

Meeting Summary

Jennifer Musselman started the meeting by introducing the project team and gave an overview of the Twiggs Street Project. The County was asked to provide concerns or known issues along Twiggs Street as well as what they had envisioned for the corridor.

Some concerns included:

- ▶ Conflict points between vehicles entering and existing Twiggs Garage in front of the Courthouse, especially if visitors to the Courthouse need to interact with bicyclists utilizing a two-way cycle track along the south side of Twiggs Street
- ▶ Fleet Vehicles are parked in the Twiggs Garage
- ▶ Uncomfortable walk for pedestrians from west of Nebraska Avenue to Channelside Drive
- ▶ Need for shade all along Twiggs Street
- ▶ Drivers' frustration waiting at long lights due to prioritized North – South movement
- ▶ Anecdotally, Rebecca and Sarah do not hear residents talking about Twiggs Street

Next Steps:

- ▶ Sarah and Rebecca to provide contacts with the Courthouse and property managers
 - John Muller and William Hand
- ▶ Communicate with Downtown Partnership and their Cleaning Green Teams
- ▶ Keep Community Infrastructure team updated as key items are decided

Tampa Hillsborough Expressway Authority (THEA)

Date: May 5th, 2023

Attendees:

- ▶ Kelly Fearon, THEA
- ▶ Anna Quinones, THEA
- ▶ Michael Garau, Kimley Horn
- ▶ Aiah Yassin, City of Tampa
- ▶ Lara Bouck, City of Tampa
- ▶ Jennifer Musselman, Kittelson & Associates, Inc.
- ▶ Jady Chen, Kittelson & Associates, Inc.

Meeting Summary

Anna Quinones started the meeting by introducing THEA's consultant Michael Garau. Jennifer Musselman then gave an overview of the Twiggs Street Project where THEA roads were concerned. THEA was asked to provide concerns or known issues along Twiggs Street as well as what they had envisioned for the corridor.

Some concerns included:

- ▶ Challenging to add gate
- ▶ Roundabout preferred at other intersections, not Meridian Avenue and Twiggs Street

Next Steps:

- ▶ Anna to provide previous study
- ▶ Anna to coordinate with Brandon
- ▶ Kimley Horn to do traffic analysis (intersection and roundabout) by end of June

Hillsborough County Facilities Management & Real Estate Services

Date: May 30th, 2023

Attendees:

- ▶ John Muller, Hillsborough County
- ▶ Kirby Jungers, Director of Security & Facilities at Court
- ▶ William (Bill) Hand, Hillsborough County
- ▶ Randy Sears, Hillsborough County
- ▶ Aiah Yassin, City of Tampa
- ▶ Lara Bouck, City of Tampa
- ▶ Jennifer Musselman, Kittelson & Associates, Inc.
- ▶ Jady Chen, Kittelson & Associates, Inc.

Meeting Summary

Aiah Yassin started the meeting by introducing the project team and Jennifer Musselman gave an overview of the Twiggs Street Project. The County was asked to provide concerns or known issues along Twiggs Street as well as what they had envisioned for the corridor.

Themes: Transportation, Security, Accessibility, Parking

General Notes:

- ▶ County owns blocks from Pierce Street to Nebraska Avenue
- ▶ Protestors in front of Courthouse add additional pedestrian, bicyclist, and vehicle conflicts
- ▶ Peak Hours of traffic around the Courthouse occur in the morning from 8am to 10am and slow down by lunch time
- ▶ 707 owns surface parking lot adjacent to the Courthouse

Major Concerns and Opportunities for Improvements

- ▶ Twiggs Garage
 - County-owned garage that is leased out to the City of Tampa
 - Consider making Garage Right In and/or Right Out Only
 - Consider bicycle and vehicle conflict at Garage driveway
- ▶ On-Street Parking
 - Parking along northern portion of Twiggs, primarily taken up by Sheriff vehicles, provide protection to the Courthouse glass façade
 - County interest to preserve on-street parking
- ▶ Crosswalks
 - Pedestrian and vehicle conflict at midblock crossing in front of Courthouse
 - Consider adding additional midblock crossing by Twiggs Garage
 - Consider updating existing crosswalks to raised crossings

- Consider adding signalized crossing to break up large block
- ▶ Sidewalks
 - Maintenance important for safety and accessibility
 - Consider moving trees to median landscaping or add curb outs
 - Consider keeping 10 feet of clearance for sidewalk (including areas with tree wells)
 - County in favor of moving utility and data lines underground
- ▶ Future Courthouse Expansion
 - Courthouse plans to expand facilities with possible plans in the southern block adjacent to Twiggs Garage or 700 Twiggs Street
 - Potential closing of N East Street

Next Steps:

- ▶ City to perform Turning Movement/Access Management Analysis for Parking Lot and Garage
- ▶ City to Coordinate with City Parking for Right In/Right Out of Twiggs Garage
- ▶ City and Project Team to continue Stakeholder interviews
 - Follow up with TECO about moving utility lines underground
 - Follow up with other providers for data lines

Appendix B: Traffic Operation Analysis